

VILLAGE OF TEQUESTA

COMMERCIAL CORRIDOR MASTER PLAN

FINAL DRAFT
OCTOBER 2022



PREPARED BY THE
TREASURE COAST REGIONAL PLANNING COUNCIL

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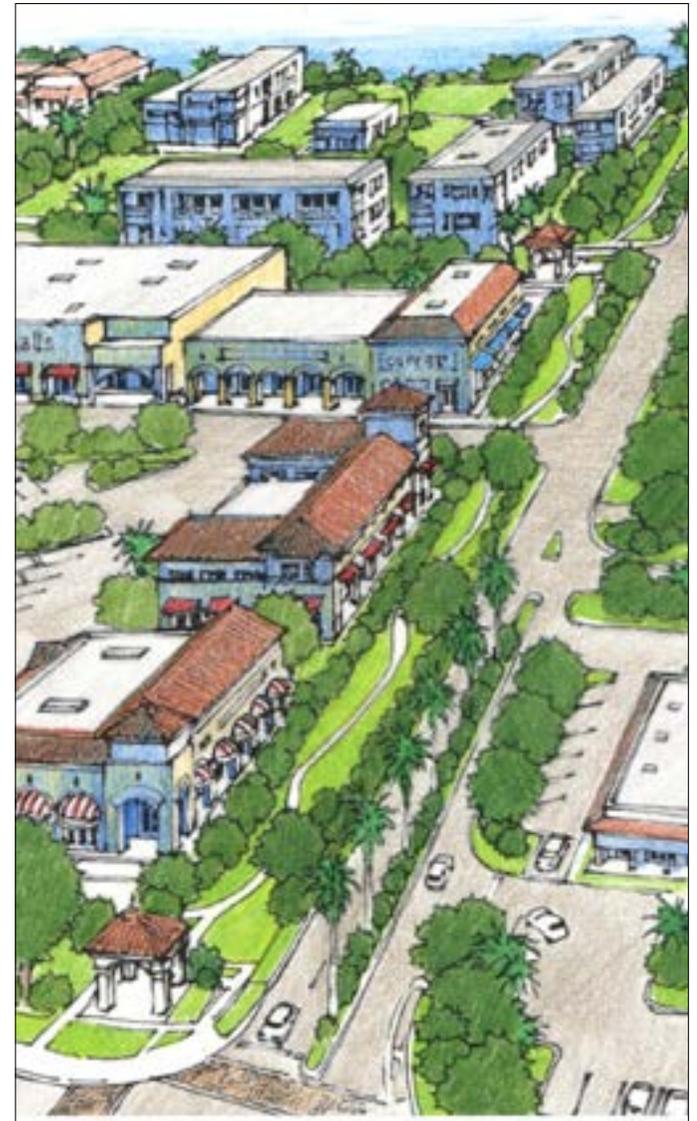
SARA BENBASAT

TABLE OF CONTENTS

- I. EXECUTIVE SUMMARY 1
- II. TOUR OF THE PLAN 2
 - GALLERY SQUARE NORTH & SOUTH
 - ROUNDBOUT
 - N. OLD DIXIE HIGHWAY
 - FEC LINEAR PARK
 - SUSTAINABILITY PARK & GARDENS
 - POST OFFICE PAVILION
 - PARADISE PARK - CITY CENTER
 - BRIDGE ROAD
 - FASHION MALL
 - TEQUESTA SHOPPES INFILL
 - TEQUESTA GREENWAY
 - FEDERAL HIGHWAY
- III. KEY RECOMMENDATIONS 35
 - LAND DEVELOPMENT REGULATIONS
- APPENDIX 53
 - A. CREATION OF THE MASTER PLAN
 - B. BACKGROUND & EXISTING CONDITIONS
 - C. CODE RECOMMENDATIONS
 - D. ARCHITECTURAL DESIGN STANDARDS

In November 2021, the Village of Tequesta (Village) entered into an agreement with the Treasure Coast Regional Planning Council (TCRPC) to conduct a public design charrette and develop a master plan establishing a vision for the commercial corridors and Mixed-Use areas within the Village. Like many other local governments in Palm Beach County, the Village of Tequesta has been experiencing increasing development pressure for residential and mixed-use development. While the Village currently has a Mixed-Use Zoning District, commercially zoned areas have also been pursued for residential redevelopment. The Village has concerns that the current zoning designations may need to be re-evaluated and updated to provide clearer direction on the community's expectations for future growth. What are appropriate building heights, building scale and positioning for new development in the Village? Are there certain incentives that should be offered in exchange for increases in public open space or the creation of Complete Streets? These questions and others were to be explored through the master planning process.

On Saturday April 9, 2022, the TCRPC team kicked off a five-day public design charrette held at the Village Council chambers. The charrette culminated two months of pre-charrette stakeholder interviews and meetings with the Charrette Host Committee to organize the logistics of the effort. On Wednesday, April 13, 2022 a Work-in-Progress presentation was given to illustrate work to date and solicit further public input. This Master Plan report illustrates the various design and regulatory recommendations made to the Village of Tequesta. Also included in the Appendix are DRAFT Architectural Design Guidelines intended to steer the architectural character of future development within the Village. It is the recommendation of the TCRPC team that this Master Plan report be adopted by the Village as a guide to future growth.



PROJECTS

This Master Plan report reflects many different ideas and concepts provided by the community and developed during the charrette process. The Master Plan is not a single “project”; the plan is a series of different projects of differing scales and levels of priority. Some of the projects illustrated within will require direct participation of the Village as they include public infrastructure or policy changes. Many of the projects described in this section are intended to be implemented by the private sector and private property owners. These concepts are provided to give guidance to future investors as to what are appropriate and desirable types of projects from the public’s perspective. Of course, not every concept will reach exact fruition, nor will there ever be complete consensus on what is “right” for the Village. This document, however, does illustrate design concepts that follow traditional planning principles and that are intended to result in beautiful, pedestrian-friendly projects that create memorable places within the Village.

The following page contains an overall map that identifies 12 different planning interventions that were identified and developed during the charrette. The objectives and design approach for each of these projects is described in some detail in the following pages. Over time, greater detail can be provided for the projects currently on the list and other projects could be added. This Master Plan document is to serve the Village for some time and should be updated periodically to assist Village elected officials, staff, and the community to articulate desired changes in the community.



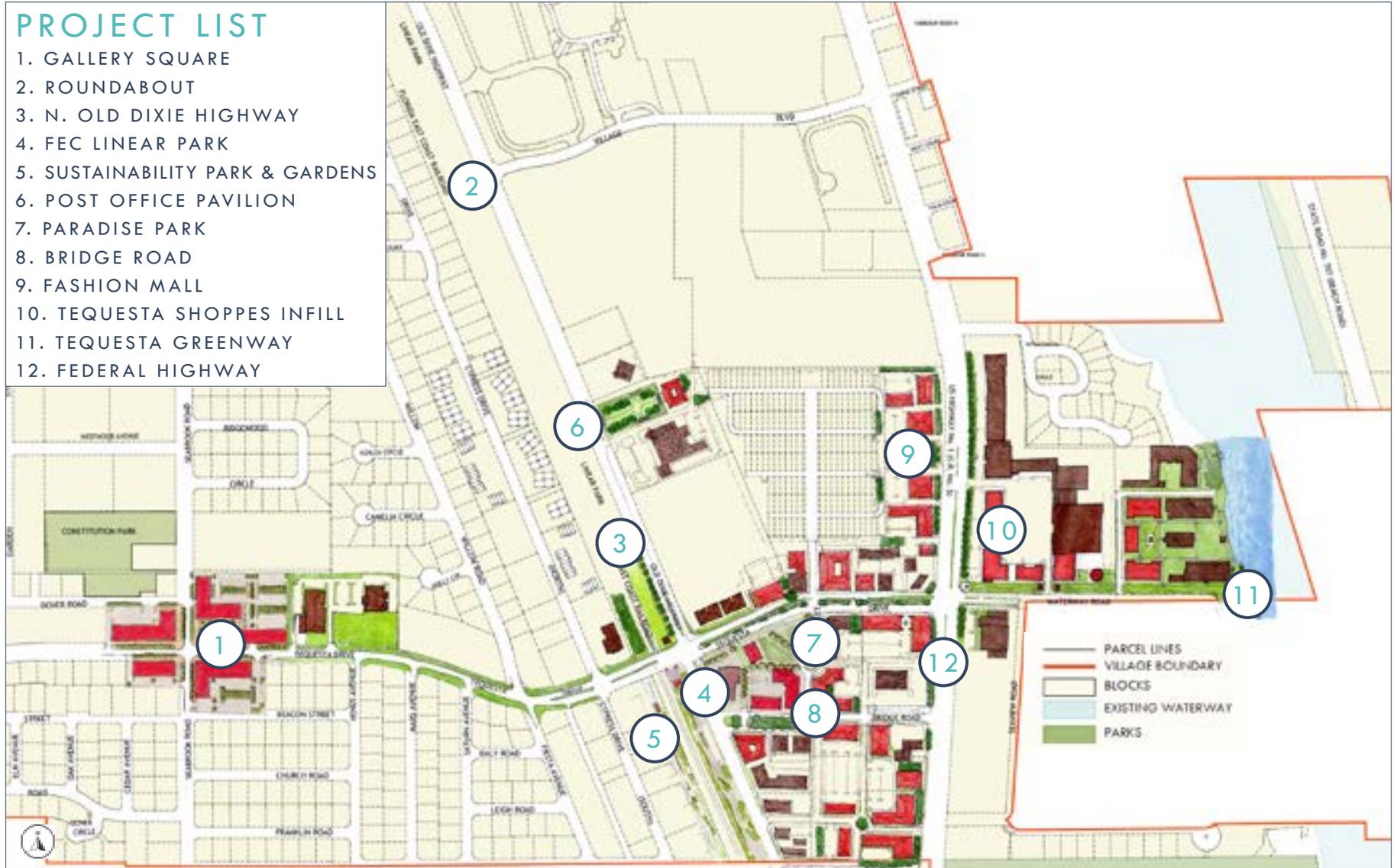
Design concept for Paradise Park inclusive of public open space.



Illustration of Tequesta’s Linear Park Trailhead concept.

PROJECT LIST

1. GALLERY SQUARE
2. ROUNDABOUT
3. N. OLD DIXIE HIGHWAY
4. FEC LINEAR PARK
5. SUSTAINABILITY PARK & GARDENS
6. POST OFFICE PAVILION
7. PARADISE PARK
8. BRIDGE ROAD
9. FASHION MALL
10. TEQUESTA SHOPPES INFILL
11. TEQUESTA GREENWAY
12. FEDERAL HIGHWAY



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1. GALLERY SQUARE
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Located in the heart of the Village at the intersection of Tequesta Drive and Seabrook Road, Gallery Square Shops (North and South) are an interesting array of local shops, businesses, and restaurants. Built during the 1960's and 1970's, the two strip shopping centers are made up of multiple individual buildings and there are multiple owners. While the two centers are very popular with the community, and provide desirable uses for Village residents, their existing configurations are typical suburban layouts with an emphasis on the large parking areas. The team chose to illustrate how over time, the centers might rebuild into a neighborhood mixed-use center with an emphasis on creating a public plaza for gathering, events, and dining. In this scenario the parking is located to the side and the rear of the buildings which face the streets and plaza.



Gallery Square proposal sketch.

GALLERY SQUARE

1

REDEVELOPMENT CONCEPT



Images of Gallery Square

II. TOUR OF THE PLAN

GALLERY SQUARE

1



The design concept also includes the parcels facing Tequesta Drive west of Seabrook Road. These new buildings would also face the street, relegate parking to the sides and rears of the buildings, and help frame the new plaza at the corner to strengthen this location as the center of town.



A particularly challenging intersection was identified by the citizen participants at the charrette: Village Boulevard and N. Old Dixie Highway. The participants were in agreement that the preferred route to Publix at the Countyline Shopping Center for most Village residents is north on Dixie Highway and then east along Village Boulevard thereby entering the plaza from the south. This route avoids having to use US-1 and make a potentially dangerous left-hand turn into the plaza. N. Old Dixie Highway is a long straight road with limited interruptions and the intersection with Village Boulevard can be dangerous. One solution discussed was to develop a roundabout at that intersection.

A roundabout would allow for the continuous flow of vehicles, force traffic to slow at that location, and would also be an opportunity for a civic monument or special landscaping. In addition, the linear park along the FEC tracks currently provides multi-purpose pathways that would be better connected to Village Parkway at that location with an appropriate intervention.



Redevelopment proposal for Roundabout Village Blvd & N. Old Dixie Hwy.

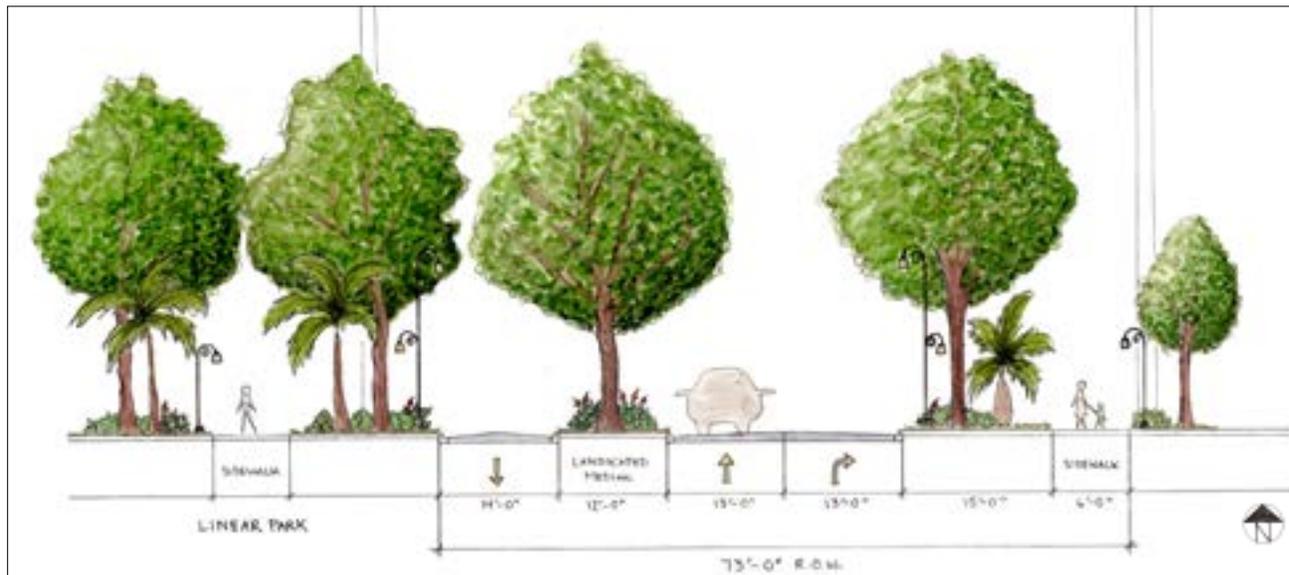
2 ROUNDBABOUT – VILLAGE PARKWAY & N. OLD DIXIE HWY

IMPROVING MOBILITY



Existing conditions of the intersection of Village Parkway and N. Old Dixie Highway

N. Old Dixie Highway is a two-to-four lane historic roadway corridor that runs parallel and adjacent to the eastern edge of the FEC Railway Corridor. There are periodic turn lanes (eastbound only) and few interruptions for the 4,700' arrow-straight stretch north of Tequesta Drive to County Line Road. As discussed earlier in this report relative to the N. Old Dixie Highway and Village Boulevard intersection, the road tends to be fast and sometimes dangerous. There are also lengths of the roadway where a center "median" is striped out with paint. It came to the attention of the TCRPC team during the charrette that the Florida Department of Transportation (FDOT) may be in the process of scheduling a RRR (Resurfacing, Restoration, and Rehabilitation) project for this section of the roadway. The team is still in the process of confirming this information and securing a potential schedule. If there is a RRR project scheduled, now would be the time to recommend some physical improvements to FDOT.



Cross section diagram of N. Old Dixie Hwy. redeveloped.

N. OLD DIXIE HWY

3

IMPROVING MOBILITY



View South along N. Old Dixie Highway

II. TOUR OF THE PLAN

N. OLD DIXIE HWY

3



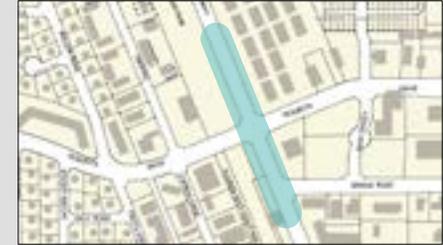
The design concept illustrated here and in Figure 3 on the previous page suggests that the center striped “median” be turned into a true landscaped median with shade trees. In addition, shade trees and missing sidewalk links should also be provided. The intent with this proposal, as is with the Village Boulevard intersection, is to slow down vehicular traffic, provide a greater level of protection for non-motorized users, and provide beauty to the corridor through landscaping and periodic structures.

This rendering illustrates that other improvements might be made along the FEC Linear Park and N. Old Dixie Highway corridor. In this rendering additional shade trees line both sides of the roadway, the previously mentioned median with shade trees is in place, and civic markers and pergola shade structures are provided as functional shelter and civic beauty along the parkway. In addition, the intersection could be raised as a speed table with textured surface treatments to slow vehicles down and remind motorists that this is an intersection used by bicyclists and pedestrians as well.

FEC LINEAR PARK – TRAILHEAD

IMPROVING MOBILITY

There currently exists an 8'-10' multipurpose pathway that is within the FEC Railway right-of-way through a lease between FEC and Tequesta and extends nearly 3,700 feet from Tequesta Drive north to opposite the Tequesta Water Treatment Plant. This is a very useful facility that could be extended and embellished over time. The trail terminates at its northern location due to FEC leasing portions of its right-of-way to Amerigas. Ideally the pathway should be extended over time to connect Tequesta Park to the system and possibly even connect to Jonathan Dickinson State Park in the future. The trail system does not extend south of Tequesta Drive and access for the continuation of the trail is impeded by the Rinker Materials Corporation facility. During the charrette, the idea of creating a “trailhead” on the south side of Tequesta Drive was discussed which could serve as a launching-off point for cyclists, runners, and hikers. This facility might include restrooms, sundries and/or vending, some parking, and outdoor shelters. The concept could work well with the idea of the Sustainability Park immediately west. This location is important not only because of the link to the linear park and pathway to the north but it also marks the transition along Tequesta Drive from more neighborhood-oriented experiences to the more urban Town Center of Tequesta.



Current conditions along the existing multi-use pathway



Existing conditions aerial of the intersection of Tequesta Drive and N Old Dixie Hwy with the existing Multi-use pathway identified in green.

II. TOUR OF THE PLAN

FEC LINEAR PARK – TRAILHEAD

4



Enhancement proposal for Linear Green trail, including Trailhead (looking South).



View of the FEC railway crossing at Tequesta Drive (looking West).



View of the FEC railway crossing at Tequesta Drive (looking East).

II. TOUR OF THE PLAN

FEC LINEAR PARK – STATION

4



Redevelopment concept for Linear Park Station.



FEC Railway view South near the intersection of Tequesta Drive.

A station plan for the FEC Railway in the Village of Tequesta was tested near the intersection of Tequesta Drive and the FEC Railway, a centrally located area of the Village.

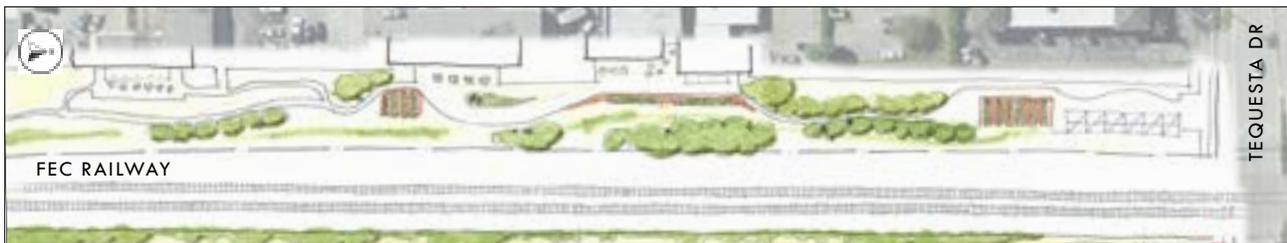
The redevelopment areas adjacent to the recommended station could develop incrementally to include a station as seen in the illustration on this page. It is also important to note the CEMEX Construction Materials company owns much of the land to the south and would need to be part of any future improvements.



SUSTAINABILITY PARK & GARDENS COMMUNITY PARKS



Existing conditions



Recommendation for sustainability park

Immediately west of the FEC Railway right-of-way, south of Village Boulevard, the Village owns an approximately 40' wide by 1,200' long strip of land behind the businesses facing S Cypress Drive. There has been some debate within the Village as to what should become of this land, "is it a public asset that needs to be preserved and improved or should it be vacated and granted to the adjacent property owners?" One idea that was discussed during the charrette was to use the land to create a Sustainability Park and Gardens. The concept is to restore this area with native plantings and habitat as a screen for the FEC rail corridor. In addition there might be limited vehicular access to the existing businesses. Another potential aspect of the Sustainability Park and Gardens could be the growing of organic fruits, vegetables, and herbs with a small restaurant or vendor structure facing Tequesta Drive. This additional use could create synergies along Tequesta Drive with the proposed Trailhead east of the FEC Railway.



Existing conditions

An idea that was not brought up by the community but did seem to have some merit was the improvement of the existing retention basin immediately north of the Post Office, south of the Tequesta Branch Library, and facing N. Old Dixie Highway. Currently the basin appears rather bleak and could be a missed opportunity for a passive park with perimeter walkways and shade trees. The plan suggests that the some of the rear parking lot of the Post Office (immediately south of the basin) be converted to a public pavilion that could have indoor and outdoor uses to better activate the basin space. The basin itself could be improved in a manner that its core function for storing storm water would not be diminished but rather, it could also serve a dual function as an asset to the area.



The Ripple Project in Martin County, FL a stormwater eco-art project partially funded by the National Endowment for the Arts. Images: Martin County CRA



EXISTING



ALTERNATIVE

POST OFFICE PAVILION

COMMUNITY PARKS



Existing retention basin

Located at the mid-point of Tequesta Drive between Old Dixie Highway and US-1 is the intersection of Main Street. Main Street, a Village right-of-way, connects to Bridge Road to the south and has a central roundabout thereby bifurcating that block. Historically this area south of Tequesta Drive was a Winn Dixie shopping plaza. In 1989 the Village hired Dover Kohl and Partners to develop a Master Plan for the area. This particular location, at the bend in the middle of Tequesta Drive, was determined to be the location for the new Town Center. Over the course of the next decade right-of-way was acquired for Main Street and the new Mixed-Use zoning was put into place. In 2001 the first of the Town Center buildings was built at 1 Main Street. Located at the SE corner of Main Street and Tequesta Drive, this 2 and 3-story, 27,979 square foot mixed-use building was the first vertical construction in the Town Center. The other three parcels adjacent to Main Street remained vacant and were ultimately leased to the Village for public use. The parcel facing Tequesta Drive was converted to parking and the two parcels facing Bridge Road to the south were improved as passive open space and a dog park.

PARADISE PARK – TOWN CENTER

7

REDEVELOPMENT
CONCEPT & COMMUNITY
PARKS



1 Main Street building on the corner of Tequesta Drive and Main Street.



Vacant parcels adjacent to Main Street



Paradise Park signage

Named Paradise Park, these open spaces grew to become important gathering places for residents, events, and visitors. While the three Paradise Park parcels (the north parking lot and the two Bridge Road open spaces) have not implemented the vision for the Town Center area, the Mixed-Use zoning still remains in place. In December 2021, the lease agreement between the Village and the property owners expired and Paradise Park is no longer available for public use. During the April 2022 public design charrette, it was made clear to the team that some form of public open space needs to be retained through redevelopment within the Paradise Park parcels. The TCRPC team focused on the importance of publicly visible, accessible, and meaningful civic open space as a core element to any development proposals for the remaining Town Center parcels. The public open space is defined with a red dashed line in the alternatives below. Part of the challenge to this Master Plan effort is to identify any policy and/or code changes necessary to ensure that the civic open space element is included.



EXISTING



ALTERNATIVE 1



ALTERNATIVE 2

II. TOUR OF THE PLAN

PARADISE PARK – TOWN CENTER

7



The TCRPC team developed a few different scenarios conceptually identifying new buildings, parking locations, and options testing the best location for the civic open space. The renderings provided illustrate some of the design options. A central plaza or square, as the civic open space, is a key organizing element for each scenario.

Rendering of proposed redevelopment for Paradise Park

II. TOUR OF THE PLAN

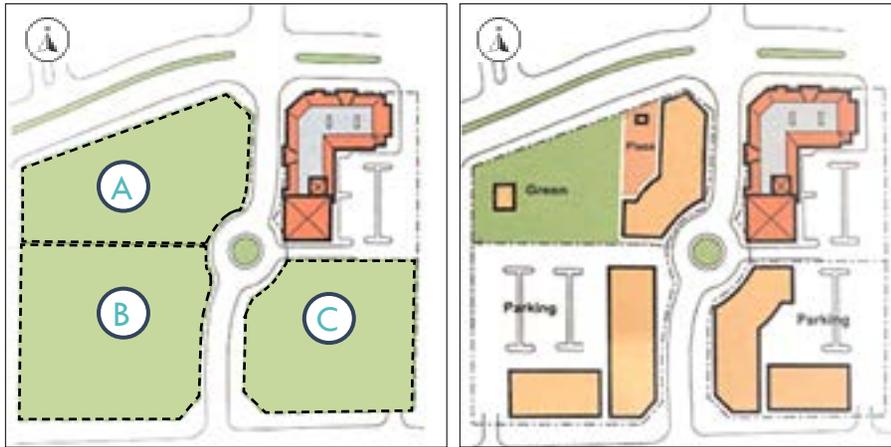
PARADISE PARK – TOWN CENTER

7

An analysis of the town center with mixed-use development resulted in this artist rendering of the quantified development strategy. The following page details the site specific land use study.



Rendering of redevelopment for Paradise Park



MIXED-USE DISTRICT ANALYSIS

Building Height: Four stories maximum of 50' high

Max Density: 18 units per acre

Minimum Landscaped Open Space: 25%

Maximum Lot Coverage: 70%

PARADISE PARK TOWN CENTER

Total: 3.17 acres

Property A: 0.98 acres (43,050 sf)

Property B: 1.18 acres (51,779 sf)

Property C: 1.01 acres (44,287sf)

Development:

- Max. Number of Units allowed: 57 Units
- Required Min. Open Space: 34,521 sf
- Max Lot Coverage: 96,660 sf
- Commercial: (1st floor) 51,000 sf
- Lot Coverage: 37%
- Residential units: (2nd/3rd floors) 55 units
- Open Space: 35,000 sf

Parking:

- 240 spaces (Commercial - 1 spaces/200 sf)
- 110 spaces (Residential - 2 spaces/per unit)

While the provision of civic open space as part of the Paradise Park redevelopment scenarios focused on Tequesta Drive, it is evident how important the Bridge Road connection is between Old Dixie and US-1. Bridge Road is an essential link in the Town Center grid system and currently is the home to many local businesses. The current conditions of the roadway, however, could be greatly improved. The right-of-way is +/- 80' and while it is only a two-lane facility, the road edges are ambiguous and vast amounts of asphalt and head-in parking give the corridor a tattered look. Through a series of street sections and 3-dimensional illustrations, the design team identified ways to formalize and discipline the corridor so that it is functional and safe for cars, cyclists, and pedestrians. In addition, the provision of a continuous canopy of shade trees will make this stretch of Bridge Road desirable as a neighborhood street.



Street diagram of Bridge Road's existing conditions

BRIDGE ROAD

8

IMPROVING MOBILITY



Existing conditions

II. TOUR OF THE PLAN

BRIDGE ROAD

8



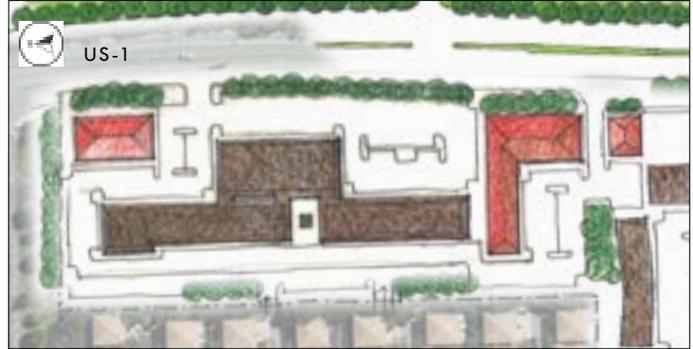
Diagram of proposed redevelopment for Bridge Road

The design proposal includes a centralized bio-swale for storm water retention and cleansing as well as pervious surfaces for on-street parking. Large inefficient head-in parking spaces could be converted into plazas and gathering spaces. Part of this conversion over time will require analyzing parking needs and potential off-site parking solutions. Wide sidewalks, on-street parking, shade trees and street lighting could over time convert this very important link in Tequesta's Town Center to a highly desirable and more environmentally sustainable location.

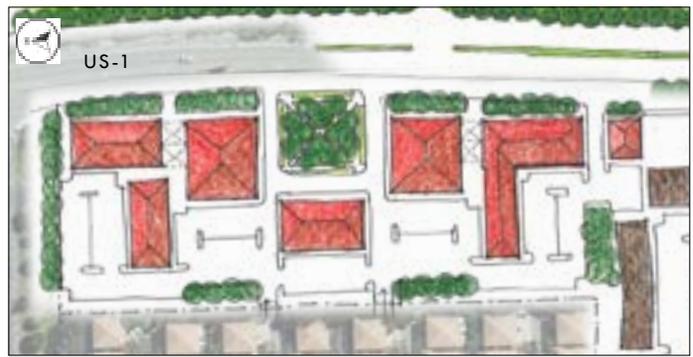
EXISTING



PHASE 1



PHASE 2

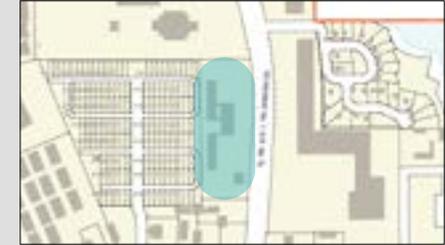


The Tequesta Fashion Mall is a commercial strip center approximately 350' north of Tequesta Drive facing US-1 on the west side. Built in 1979, the 4.24 acre, 37,653 square foot center contains a couple of popular local restaurants, local retailers, and neighborhood services. The Tequesta Fashion Mall also has approximately 740' of direct frontage on US-1, is fairly well connected to the commercial parcels to the south, and has the potential over time to become an extension of the Town Center area on Tequesta Drive.

Although the Fashion Mall has a few very popular uses, there was consensus that this site be considered for wholesale future redevelopment. The buildings are over 40 years old, their configuration is odd and dated, and there is a great opportunity to create a memorable mixed-use project on US-1 that serves as a gateway into the Town Center area on Tequesta Drive.

FASHION MALL

REDEVELOPMENT CONCEPT



Existing conditions

II. TOUR OF THE PLAN

FASHION MALL

9



RESIDENTIAL



OUTDOOR DINING



OUTDOOR DINING



MIXED-USE

The design concept provided is organized around a center green which could be lined with on-street parking and provide spill-over seating for restaurants and display space for shops. The building frontage north and south of the central green would engage an expanded sidewalk along US-1 similar to the images provided on this page.



Considering that the Tequesta charrette was held after nearly a year and a half of pandemic quarantine which had a significant impact on retail and restaurants, it is remarkable how few retail/restaurant vacancies are in the Village of Tequesta. While this is very positive for the Village, it did make choosing potential redevelopment sites to test design concepts more challenging as none of the centers are in obvious distress. The Tequesta Shoppes became a candidate for incremental infill along US-1. Built in 1986, this 110,620 square foot plaza is a standard suburban model consistent with its age. The major tenants include Marshalls, HomeGoods, and the Dollar Store; all of which seem to be very popular with the community. Unfortunately, the massive parking lot does little to engage or improve the physical environment of US-1. This design concept proposes to infill the first few bays of the angled parking rows and create new structures in line with the shops at the north end of the plaza. This is depicted in red on the concept plan on the left. The head-in parking and travel lanes nearest US-1 would remain intact however the inline shops would provide a greater sense of spatial enclosure along the corridor as well as conceal the remaining large surface parking field.

TEQUESTA SHOPPES

10

REDEVELOPMENT CONCEPT

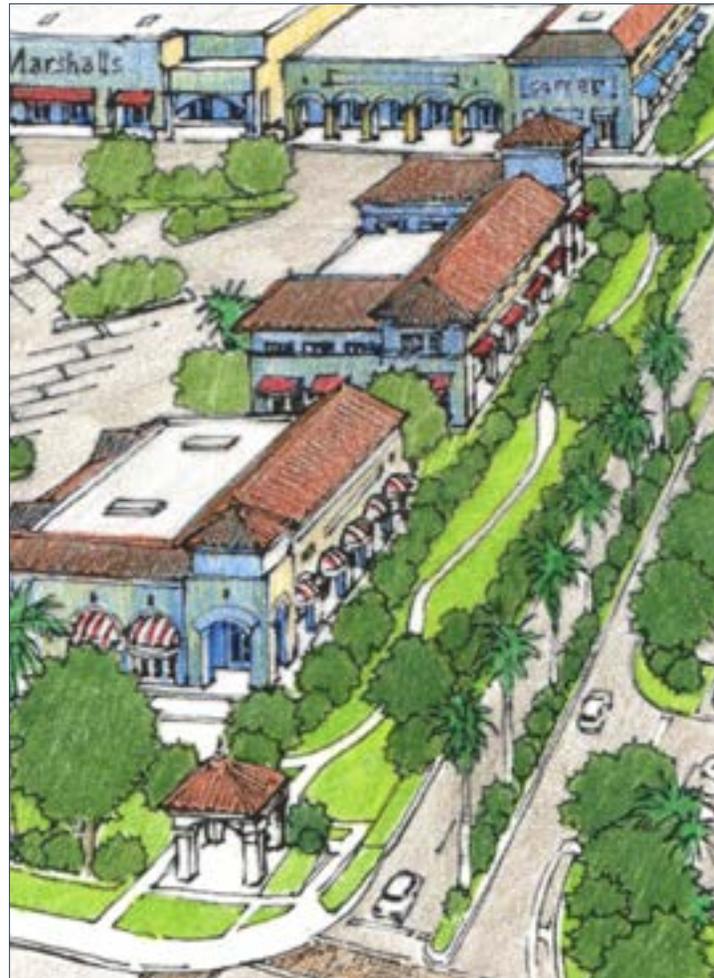


Existing conditions

II. TOUR OF THE PLAN

TEQUESTA SHOPPES

10



This treatment of new infill development is slightly different from what is proposed at Tequesta Fashion Mall in that the existing head-in parking remains and the new buildings do not engage the street and sidewalk directly. This approach is taken for the Tequesta Shoppes in order to maintain the existing connectivity to the existing shops to the north and to provide continuity along the street. If there was a proposal to raze the entire center and completely redevelop the site, the approach would have been more consistent with The Fashion Mall proposal. In addition to the new retail frontages, this concept also illustrates a new civic or public building with a small square facing US-1.

TEQUESTA GREENWAY

REDEVELOPMENT CONCEPT & COMMUNITY PARKS



Existing conditions view looking East along Waterway Road



A longer-term design concept is the Tequesta Greenway - a continuous park-like environment connecting US-1 to the Intracoastal Waterway along the north side of Waterway Road. Waterway Road intersects with US-1 and is the eastward extension of Tequesta Drive tying into the heart of the Village. The areas identified for the future greenway include parking and drive aisles for the Trust Bank Branch, the office building at 20 Waterway Road, some parking for the Tequesta Shoppes, and portions of the Waterway Beach Condominiums. Currently the only sidewalk along Waterway Road is along the Trust Bank parcel however the further east one travels along that corridor the more open space (public and private) that exists for connection opportunities.

Initial phases could include extending the sidewalk east to where Waterway Road turns south. Providing safe pedestrian passage might increase the likelihood that residents will walk to shops or restaurants (it is less than ¼ mile, or a 3-minute walk, from Waterway Condominiums to US-1). Over time, as parcels redevelop or there are changes of use requiring fewer drive aisles and parking spaces, the sidewalk connection along the north side of Waterway Road could expand in larger green areas with shade trees and street lighting.

TEQUESTA GREENWAY

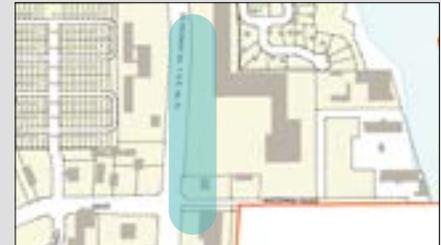
11



Ultimately, if there are any substantive changes to, or redevelopment of, the Waterway Condominiums, there might be an opportunity to provide public access to the waterfront. This would be an excellent opportunity to provide paddleboard or kayak portage with bicycle parking. As mentioned, this is a long-term vision but over time every opportunity to improve this connection should be seized upon.

US-1 HWY COMPLETE STREET

IMPROVING MOBILITY



Existing conditions along US-1

In January 2015 the Village of Tequesta, in collaboration with the Treasure Coast Regional Planning Council, kicked-off a public planning process to look at the potential of transforming US-1 (at that time a 6-lane corridor) into a more “Complete Street”. With supportive data and analyses from a traffic study and community input, the proposal was to eliminate the outside travel lanes in each direction and replace them with buffered bike lanes. In addition, street lighting, landscaping, and wider sidewalks were included in the proposal, with the existing curbs remaining in place.

The project was completed in 2019 and almost immediately became a source of contention for some within the community. Despite being an award-winning urban design project, many vocal motorists were opposed to the change. There were also many in the community, particularly those residents who embrace change within the Village, who saw the benefits of slowing down traffic, providing bicycle facilities, and reducing the visual impacts of a six-lane facility as the front door to Tequesta. There continues to be debate over whether or not to return the roadway to its previous six-lane section or continue the improvements that were never fully implemented.

During the Tequesta Master Plan charrette and interview process, the discussion of US-1 continued. Having been a few years since its completion, there was an opportunity to assess the benefits/challenges of the US-1 project a bit more objectively. There are still residents and users of US-1 that do not support the road modifications however, there are many residents and users that not only want to keep the current configuration but enhance the improvements further. Charrette discussions regarding US-1 improvements focused mainly on providing more shade trees and over time converting the buffered bike lane to a separated multi-purpose pathway.

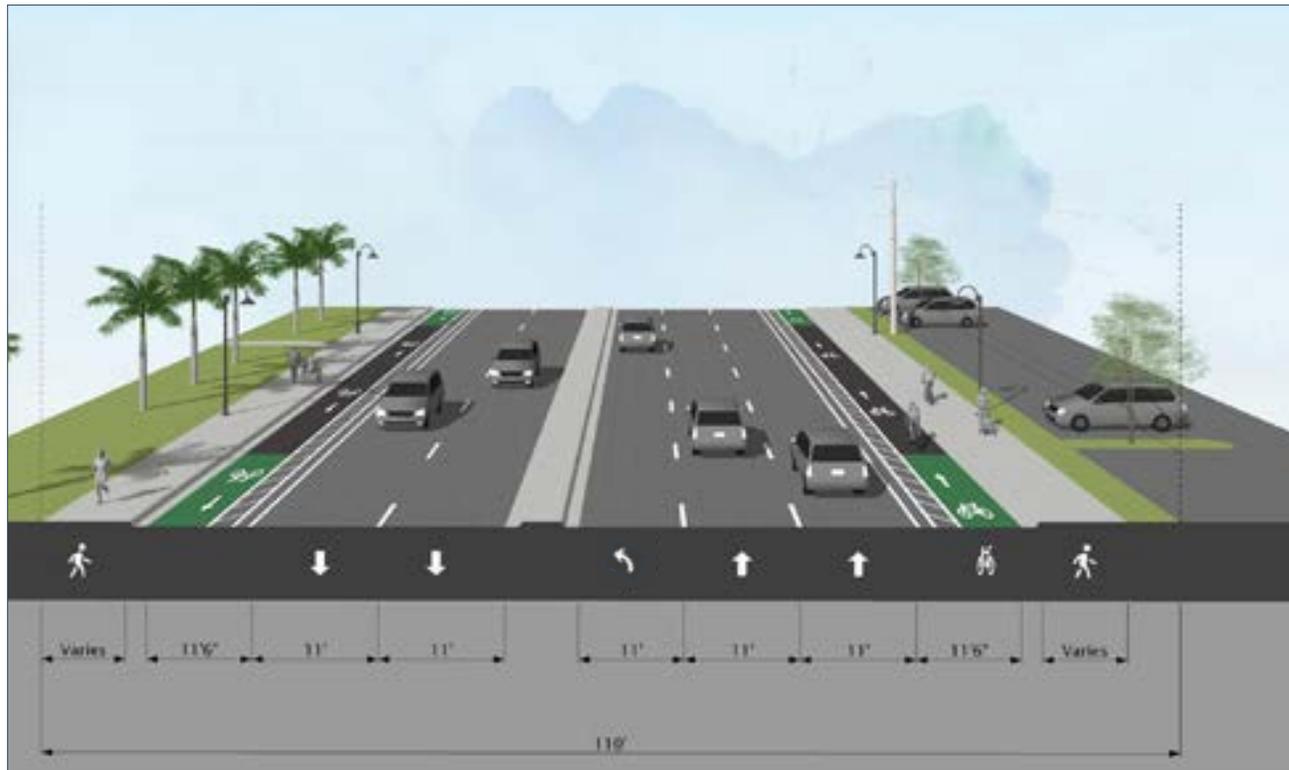
The following street sections illustrate the current and proposed conditions.

CURRENT CONDITIONS:

This section shows US-1 as it is today: 4 travel lanes in each direction, buffered bike lanes on either side, and some landscaping where it can be accommodated, typically on private property and more often than not, palm trees versus shade trees. This section is taken immediately south of Tequesta Drive looking northbound.

US-1 HWY
COMPLETE STREET

12



PHASED I PROPOSED IMPROVEMENTS:

Working with private property owners fronting US-1, the Village should identify locations where new shade trees can be provided. In some cases the removal of some existing parking spaces could provide sites for plantings. The Village should take the lead on developing a program to fund the improvements, provide the trees, and certify maintenance and watering for a designated time period.

US-1 HWY COMPLETE STREET

12



BEFORE



AFTER



TREE PLANTING PROGRAM

Throughout the planning process the US-1 corridor lane re-purposing project from 2015 was discussed at length. The general consensus is that the street is not fully “complete” and would benefit from additional shade trees. In fact, the provision of additional shade trees is a common theme throughout the Village.

The existing US-1 cross section is completely built out leaving no additional room for the desired tree canopy. One idea is to develop a public-private tree planting program whereby the Village collaborates with private property owners to provide shade trees on their property with assistance from the Village. The diagram to the left shows how new trees might be provided in the Tequesta Shoppes plaza by removing a few parking spaces and replacing with tree wells.

Recommended tree species can be found on the following pages.

US-1 HWY COMPLETE STREET

12



Existing conditions view north along US-1 at Tequesta Shoppes.



LIVE OAK *QUERCUS VIRGINIANA*

ATTRIBUTES

- Grows rapidly when young. Can live for centuries. Adapts to almost any type of soil. Tolerates salt spray and compacted soil.
- Works well as a street tree. Has superior wind resistance.
- Assumes a dwarf form on drier sites.
- Grows in a rounded shape. A live oak tree is a moderate grower, it needs full sun and room to spread its wings, as well as a well-drained planting location. Oaks are cold hardy, and fine in any Florida planting zone.
- Trimming is unnecessary for a young live oak tree but watering is critical. These trees must have regular irrigation to grow strong root systems that will support this large a tree.
- Clear zone from sidewalks of 10 feet or more so roots don't cause problems.
- Avoid placing near other big trees that will shade it.



LIVE OAK <i>QUERCUS VIRGINIANA</i>		
	Native or Florida Friendly	Yes
	Zone	Zone 7-10
	Height and Canopy	Height of 40–80' and a spread of 60–100' at maturity
	Growth Rate	Medium rate, with height increases of 13–24" per year
	Drought Tolerant	Has some flood and drought tolerance
	Soil preference and Maintenance	Very adaptable—growing in most soils
	Flowering/Fruiting	Produces green acorns
	Fruit/Leaf Litter	Some leaf litter with new growth
	Wildlife Value	Acorns provide food for many birds and mammals

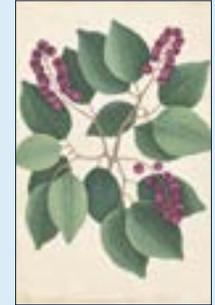


PIGEON PLUM *COCCOLOBA DIVERSIFOLIA*

ATTRIBUTES

It is a Florida native with a nice shape and attractive exfoliating bark. Pigeon Plum also appears on most city-approved tree lists. The Pigeon Plum is very similar in appearance to its Sea Plum and Sea Grape relatives.

- Does well in full sun or part shade. Moderately fast growing in part shade, but in full sun, this plant is thought to be fast growing.
- It throws purple fruit in which birds enjoy eating. Its sweet fruit attracts birds, such as the mockingbird, robin, pigeon, dove, and woodpecker, creating a nice natural habitat for indigenous wildlife. It only drops for 2 months out of the year.
- No need for irrigation after becoming established. It gets a dense and compact canopy.
- This tree is both drought and salt tolerant.
- High wind tolerance makes this a hurricane resistant tree.



PIGEON PLUM <i>COCCOLOBA DIVERSIFOLIA</i>		
	Native or Florida Friendly	Yes
	Zone	Zone 10b-11
	Height and Canopy	Height of 15-25' and a spread of 20-35' at maturity
	Growth Rate	Medium rate, with height increases of 12-36" per year
	Drought Tolerant	High drought tolerance
	Soil preference and Maintenance	Moist well-drained soils
	Flowering/Fruiting	Producing year-round flowers/fruiting only once a year
	Fruit/Leaf Litter	Dropping fruit only two months out of the year
	Wildlife Value	Sweet fruit attractant to birds





GREEN BUTTONWOOD *CONOCARPUS ERECTUS*

ATTRIBUTES

A Florida native, buttonwood is ideal for seaside plantings. It withstands the rigors of urban conditions very well and makes a durable street or parking lot tree.

- Due to its small size, plant on 15-foot centers to form a closed canopy along a street.
- Purchase single-trunked trees for street and parking lot plantings.
- It is highly tolerant of full sun, sandy soils, and salty conditions.
- It also tolerates brackish areas and alkaline soils, thriving in the broken shade and wet soils of hammocks.



GUMBO LIMBO *BURSERA SIMARUBA*

ATTRIBUTES

Gumbo limbo is ideal for a freestanding specimen on a large property or as a street tree. Lower branches will grow close to the ground, so street trees will have to be trained early for proper development. Locate the lowest permanent branch about 15 feet off the ground to provide enough clearance for a street tree planting.

- Moderately fast growing, with great resistance to strong winds, drought, and neglect. Drought avoidance is accomplished by leaf drop, and growth is often best in drier locations not receiving irrigation.
- The inconspicuous flowers are followed by red, three-sided berries that split into three sections at maturity to reveal a 1/4-inch triangular red seed. The fruit takes a year to ripen and matures in early summer.
- Specimen trees are often grown with branches beginning much closer to the ground, providing a beautiful specimen plant with wonderful bark.



GREEN BUTTONWOOD *COCOCARPUS ERECTUS*

	Native or Florida Friendly	Yes
	Zone	Zone 10b-11
	Height and Canopy	Height of 30-40' and a spread of 20-30' at maturity
	Growth Rate	Moderate growth rate
	Drought Tolerant	High drought tolerance
	Soil preference and Maintenance	Very adaptable—growing in most well-drained soils
	Flowering/Fruiting	Producing non showy flowers/fruiting year round
	Fruit/Leaf Litter	No
	Wildlife Value	Host plant and nectar source for rare butterflies and moths



GUMBO LIMBO *BURSERA SIMARUBA*

	Native or Florida Friendly	Yes
	Zone	Zone 10b-11
	Height and Canopy	Height of 20-60' and a spread of 20-50' at maturity
	Growth Rate	Rapid growth rate
	Drought Tolerant	High drought tolerance
	Soil preference and Maintenance	Very adaptable—growing in most well-drained soils
	Flowering/Fruiting	Producing flowers/fruiting in the Spring
	Fruit/Leaf Litter	No
	Wildlife Value	Flowers are rich in pollen and fruit is attractant for a variety of birds



KEY RECOMMENDATIONS & IMPLEMENTATION

The success of this (and any other) Master Plan will depend on its ability to be implemented economically and socially within a designated time frame. To that end, the recommendations throughout this report have been developed as independent but interrelated projects. Some, such as public infrastructure projects, are within the Village's control to pursue implementation, with funding being the primary challenge. Other recommendations are redevelopment techniques that are illustrated on private property - which are subject to each private entity's time frame and financial situation. In order to realize these types of projects, the principles of urban design described and illustrated through the report and in the examples have to be embedded culturally within the Village, required by its codes, and encouraged through its programs. The combination of public and private efforts is required for realizing the vision of the Master Plan.

LAND DEVELOPMENT REGULATIONS

Updating the existing Land Development Regulations (LDRs) in a few key areas will be important in ensuring that the community vision is more predictably attainable. During the public design charrette a number of code elements were identified as needing attention. LDRs are intended to both protect existing residents and businesses from impacts of adjacent development and to ensure a desirable physical form. LDRs can also serve as a redevelopment tool - providing critical information to potential investors and a clear approval process. If it is difficult to ascertain what can be built and how long the process takes, codes can function as a disincentive for redevelopment.

During the design charrette and in meetings with staff the following code items have been identified for potential code revisions.



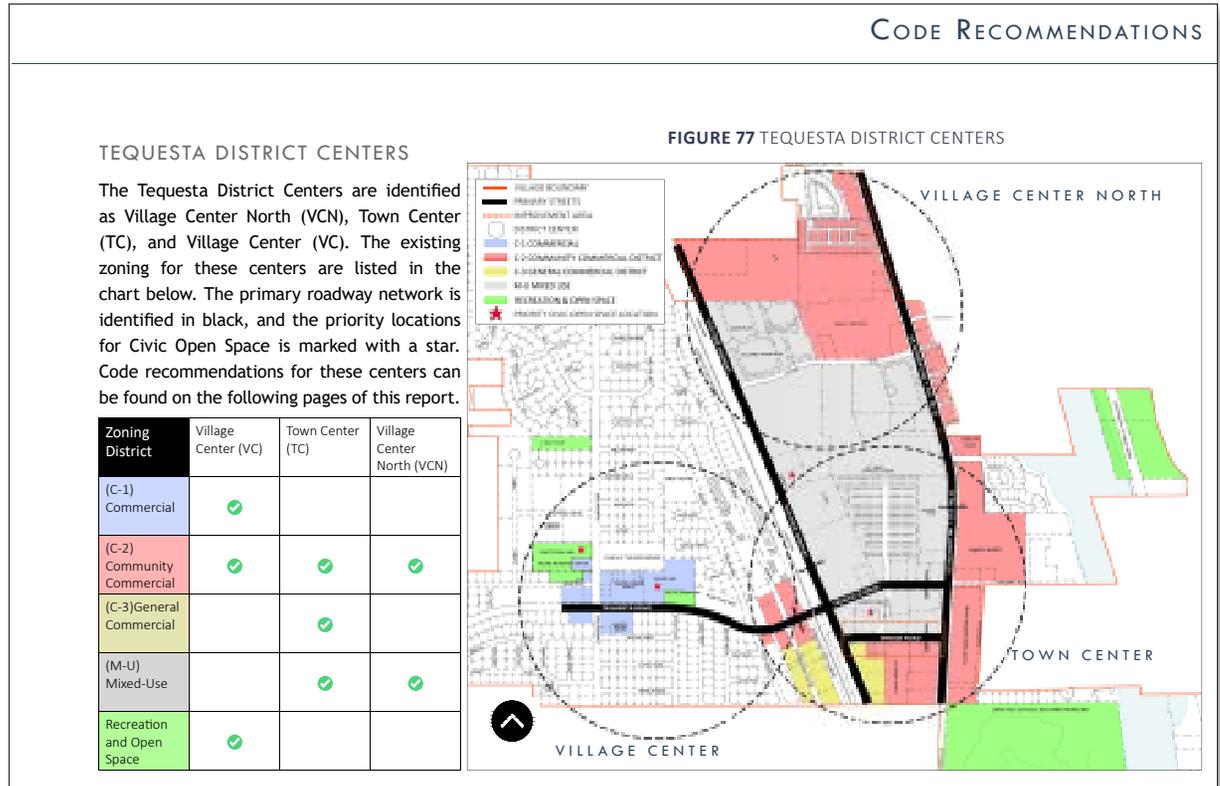
CODE RECOMMENDATIONS

DISTRICT CENTERS

In an effort to provide specific LDR-type recommendations in a contextual manner, three different Districts have been identified as part of this plan: Village Center District, Town Center District, and Village Center North.

Each of these areas has unique characteristics and their geographical boundaries, as defined by a 1,350' (5 minute) walking radius, are illustrated on the Regulating Plan to the right. The Regulating Plan is intended to be supplemental to the existing Zoning Map. The Regulating Plan identifies the District Centers, Primary Streets, locations of Civic Importance, and locations where detailed street sections and frontage diagrams are defined.

The following pages are an overview of the more complete and detailed Code Recommendations provided in Appendix C.



Excerpt from the Code Recommendations found in **Appendix C**.

CODE RECOMMENDATIONS

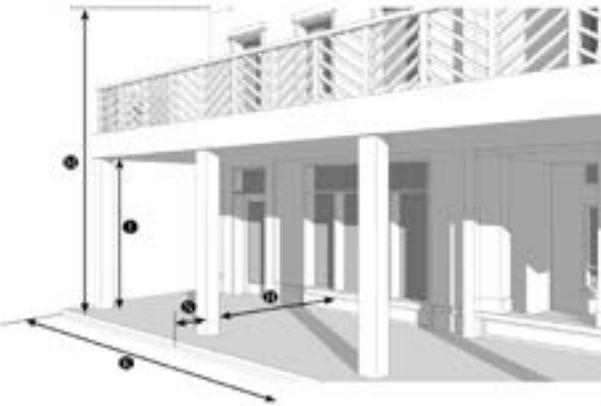
FRONTAGE STANDARDS

Nearly every building has a front and a back. It is rare that all four sides of a building are treated with the same level of finish and design, nor are they typically approached from all sides. Typically it is Civic Buildings that have multiple “fronts”.

The Frontage Standards defined in Appendix C are intended to provide specific architectural details and critical dimensions for differing types of building fronts. Some offer arcades, some offer storefronts, and others provide courtyards. Not all building frontage types are appropriate in all places. The more detailed code recommendations in Appendix C identify the appropriate locations for different building frontages in the different Center Districts.

APPENDIX C

FIGURE 42 ARCADE FRONTAGE



Description

An arcade is a covered, unglazed, linear hallway attached to the front of a building, supported by columns or pillars. The arcade extends into the public right-of-way over the streetscape area, creating a shaded environment ideal for pedestrians. This frontage type is typically associated with commercial uses. Arcades that remain open to the public at all times, in the case where an arcade encroaches into the public right-of-way, a right-of-way maintenance agreement may be required.

Dimensions

Depth, clear	8 feet min.	+
Height, ground level clear	10 feet min.	+
Width, length of facade	70% min.	+
Finish level above finished grade	at sidewalk level	-
Height, stories	2 stories max.	+
Set back from curb	2 feet min. / 4 feet max.	+

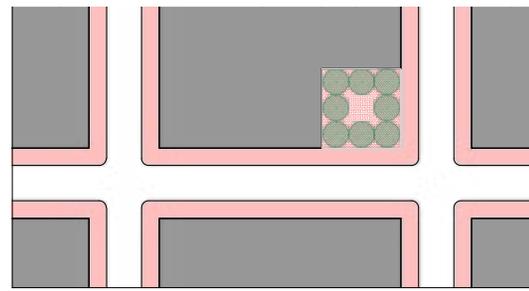


Excerpt from the sample Frontage Type Standards found in Appendix C.

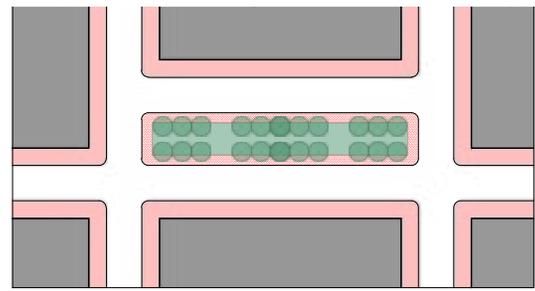
CODE RECOMMENDATIONS

CIVIC OPEN SPACE

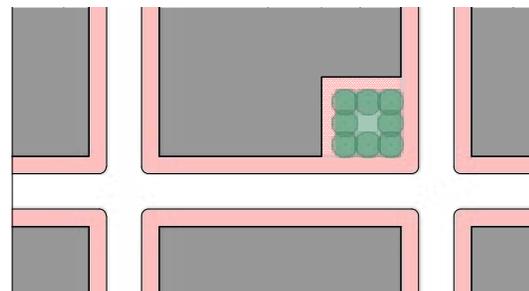
The current regulations for the M-U Mixed-Use and C-2 Community Commercial zoning districts require 30% of a development area be set aside for open space. Often those requirements are satisfied through setbacks, buffers, and residual landscape areas such as parking islands. This plan recommends that the open space requirements be more clearly defined as usable civic open space in the form of parks, greens, and public squares. The included alternative civic open space configurations diagram is a potential tool to more clearly define how future development civic open space requirements should be delivered.



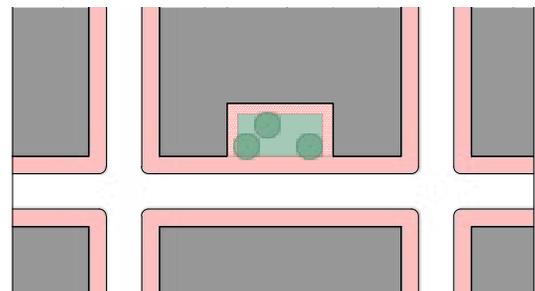
PLAZA



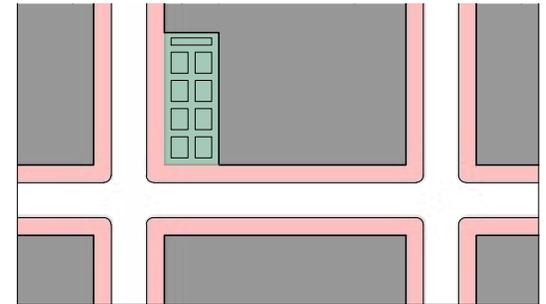
SQUARE



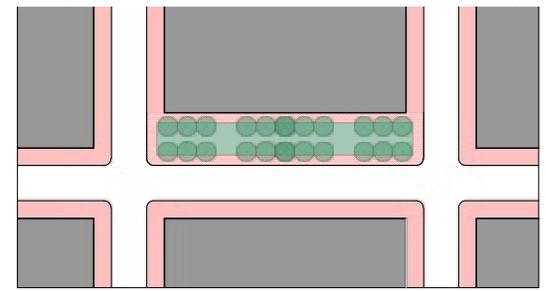
GREEN



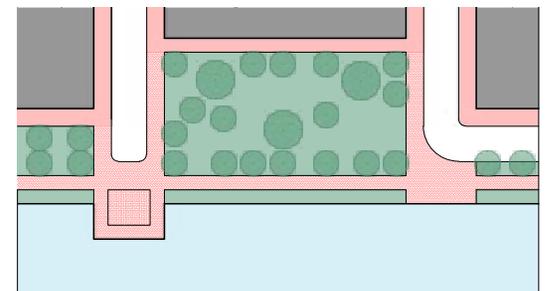
COURTYARD



COMMUNITY PLAYGROUND



ATTACHED GREEN



WATERFRONT GREEN

III. KEY RECOMMENDATIONS & IMPLEMENTATION

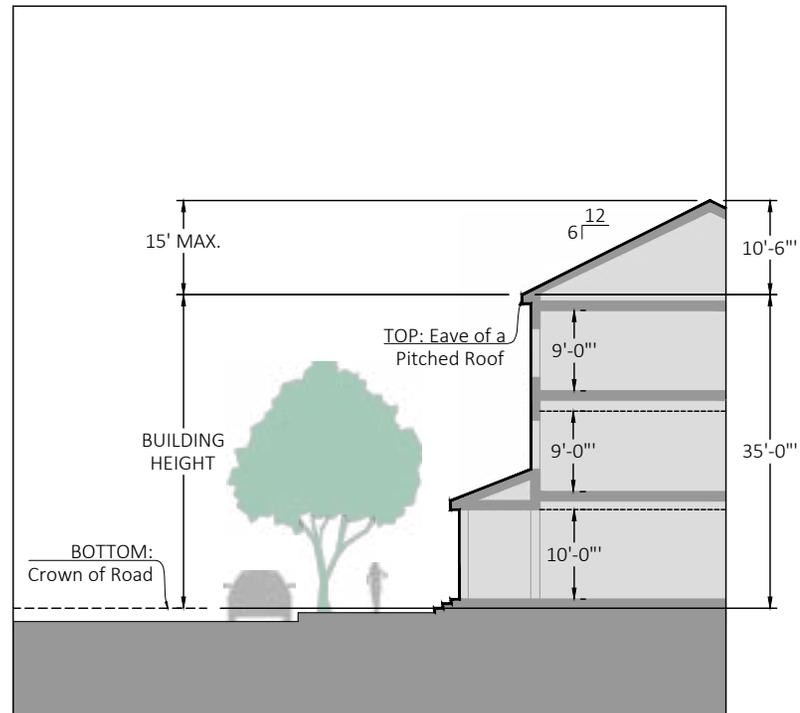
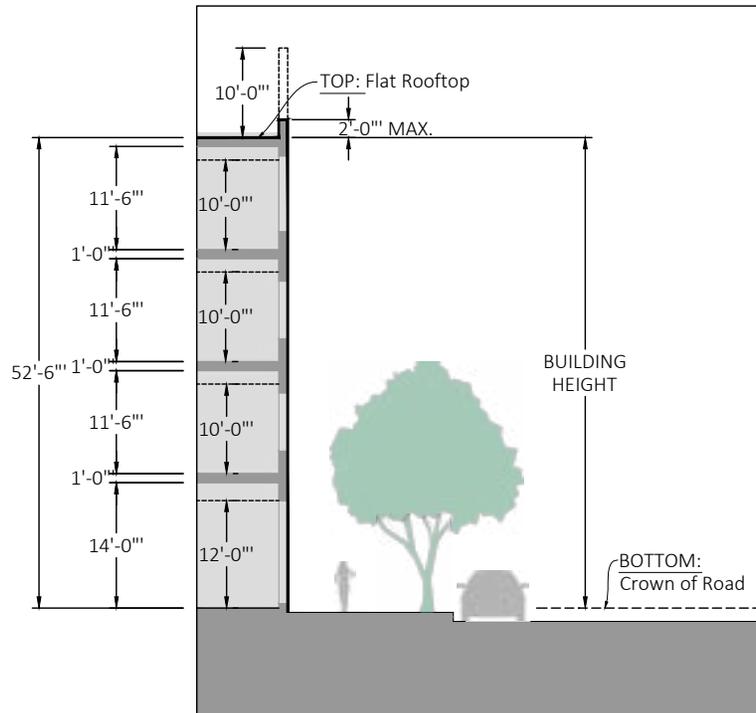
FIGURE 01 OPEN SPACE CONFIGURATIONS DIAGRAM

	SIZE	DESCRIPTION	ILLUSTRATION
SQUARE	≥ 10,000 sf	A square adjoins streets on at least three sides. Squares may be up to 50% hardscaped, with formal landscaping and shade trees. Squares accommodate both passive uses and community gatherings.	
ATTACHED GREEN	2,000 to 6,000 sf	An attached green spans the entire length of a block. Attached greens shall be at least 30 feet wide and are appropriate on the short end of a block. Attached greens are primarily lawns with formally arranged landscaping and shade trees.	
COURTYARD	Courtyard space ratio of height to width: Min. = 1:1 Max. = 1.5:1	An uncovered area for pedestrians partly or wholly enclosed by buildings or walls and used primarily for supplying access, light, and air to abutting buildings.	
GREEN	1,000 to 5,000 sf	A continuous area for pedestrians which is open from the ground level to the sky for its entire width and length, the primary feature of which is a landscaping scheme that incorporates garden elements including trees, palms, shrubs, or ground cover, as well as water elements including a fountain or pond.	
PLAZA	1,000 to 43,000 sf	Fronts on the street and is directly accessible to the public at all times for use by the public for passive recreational purposes. The ground level of the plaza shall be reconstructed principally of hard-surfaced materials. An existing unimproved area between or next to a building or buildings shall not qualify. Should not be near another plaza.	

CODE RECOMMENDATIONS

MEASURING BUILDING HEIGHT

Currently there is a maximum four-story, 50'-0" building height in the Village. One LDR recommendation is that less emphasis be placed on the maximum height in feet and inches but instead focus on number of stories. The diagram below offers recommendations for where building height measurements should be taken (top of roof deck or the roof eave). In addition the diagram to the lower left shows how more generous modern floor-to-ceiling heights can exceed 50'-0".

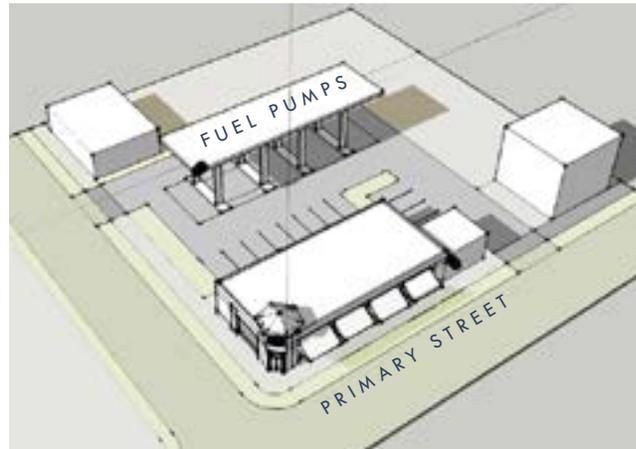


CODE RECOMMENDATIONS

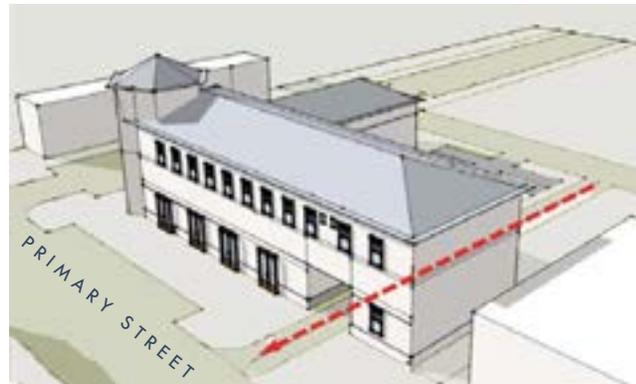
DRIVE-THROUGH BUILDINGS & GAS STATIONS

The provision of commercial drive-through environments in the MU and C-2 districts was brought up repeatedly during the public design charrette as an area needing attention. There are community concerns that allowing drive-throughs can erode the public realm and impact the potential for multi-modal environments along the corridors. One approach would be to simply prohibit drive-throughs, however there are other physical design requirements that can accommodate drive-throughs and maintain urban building frontage continuity along the street. The following diagrams illustrate different ways drive-throughs can be accommodated in an urban environment.

Limiting drive-through access from Primary Streets (as defined in the District Regulating plans) and only allowing access from side streets or driveways will lessen the impact to the public realm from the drive-throughs. In addition, fuel pumps at filling stations should be located to the interior of the site and screened from view from the Primary Street.



"REVERSE" GAS STATION



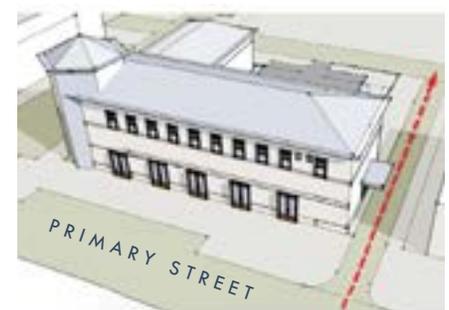
DRIVE-THROUGH CONFIGURATION 1

Circulation and stacking travel through the building. Cars enter from the rear of the lot and exit to the primary street.



DRIVE-THROUGH CONFIGURATION 2

Drive-through stacking occurs in the rear of the lot. Circulation is from the rear of the lot, exiting to a non-primary street.



DRIVE-THROUGH CONFIGURATION 3

Circulation and stacking occurs along the side of the building. Cars enter from the rear of the lot, exiting to the primary street.

CODE RECOMMENDATIONS

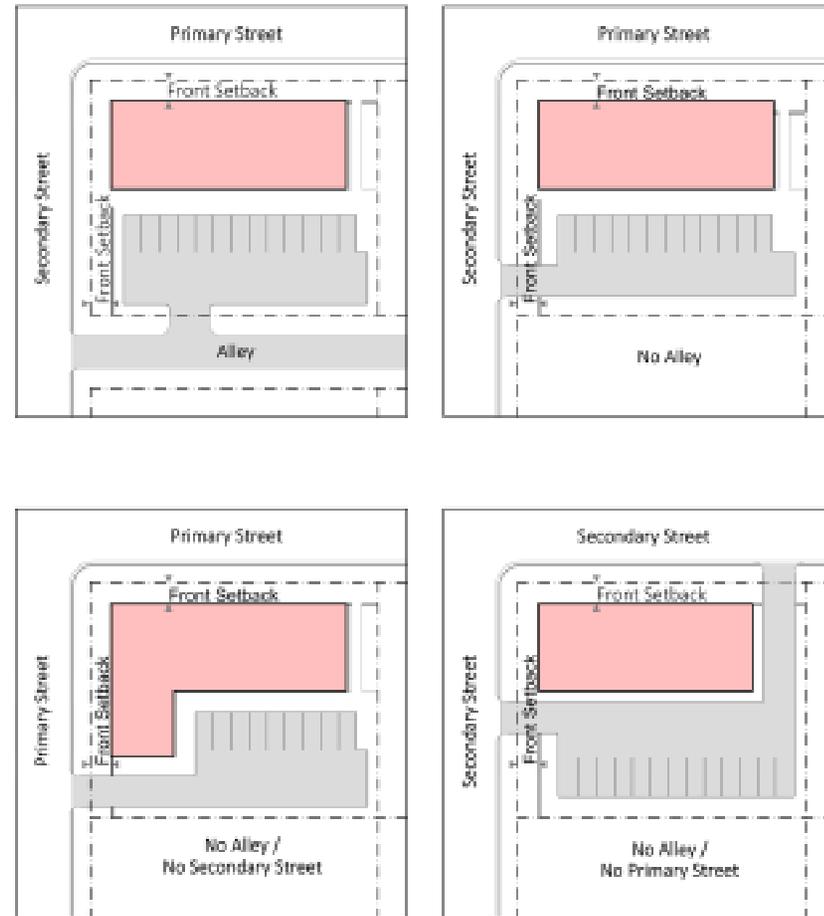
PARKING LOCATIONS

The provision of adequate vehicle parking is an essential component of maintaining healthy and vibrant business environments. The quantity and location of on-site parking requirements can also become a detriment to pedestrian environments and the quality of a place. Conventional standards of front-loaded parking lots, and the requirement of an over-abundance of parking spaces, has resulted in the degradation of the public realm and made the pedestrian and non-motorized environments challenging.

Updates to the LDRs should specifically identify the appropriate locations of parking areas - namely to the back and to a limited extent, the side of buildings. Parking standards should also recognize the inherent efficiencies of a single or inter-connected parking lot(s) serving multiple uses and businesses and allow a reduction in the number of required spaces. The Master Plan identifies a number of design concepts that illustrate this approach. Parking lots should not be the most prominent feature of future redevelopment.

The diagram to the right identifies preferred parking locations depending upon different site and Primary Street considerations.

FIGURE 02 PARKING LOCATIONS

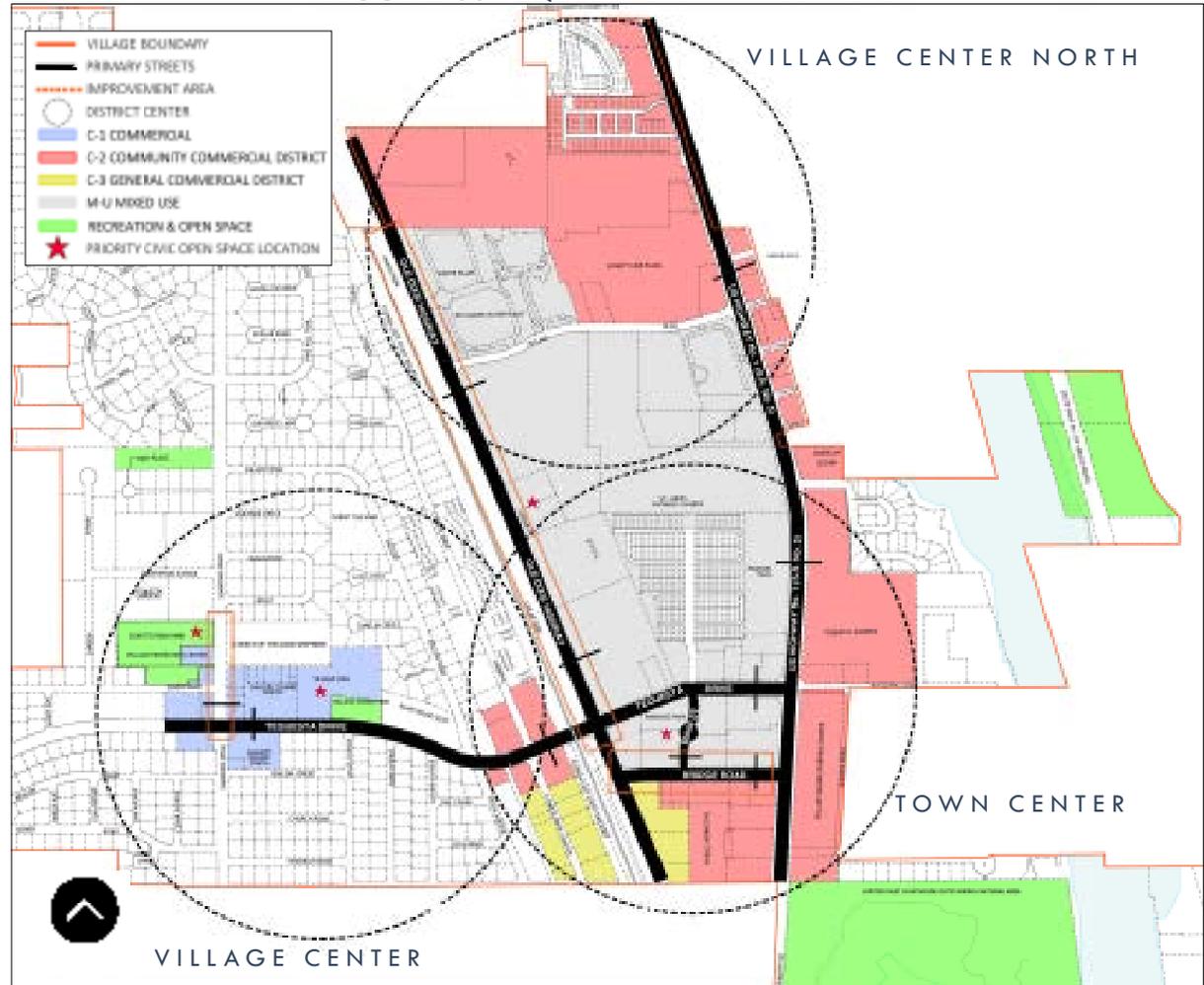


CODE RECOMMENDATIONS

PRIMARY STREETS

A helpful tool in ensuring the creation and preservation of walkable and desirable urban conditions is identifying Primary and Secondary roadway networks. Primary streets are those with the greatest visibility and potential for positive redevelopment impacts in creating healthy urban experiences. These corridors should be held to the highest urban design standards. Non-Primary streets are those roadways where greater flexibility is allowed and the provision of parking lots and/or garages would be permitted. The Primary roadway network is identified in black in the diagram on this page.

FIGURE 03 TEQUESTA DISTRICT CENTERS



CODE RECOMMENDATIONS

TEQUESTA IN HARMONY WITH THE WILDERNESS

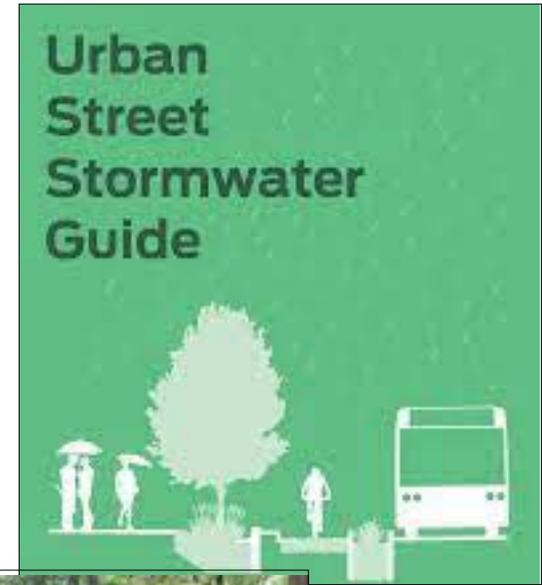
Tequesta is uniquely situated between important habitats such as the Loxahatchee River, Johnathan Dickinson State Park, the Intracoastal Waterway, and the Jupiter Lighthouse. The following are policies that could be incorporated through grants, programs, polices, and Land Development Regulations to further enhance the relationship between development and the environment.

GREEN STORMWATER INFRASTRUCTURE

Green stormwater infrastructure, or low-impact infrastructure, practices and policies reexamine traditional engineering approaches to stormwater management by integrating stormwater management within the right-of-way and within the development in a compact and often beautiful landscape plan. Rain gardens, tree boxes, and pervious surfaces are utilized to keep and treat stormwater at its source instead of relying solely on conveying stormwater rapidly off-site.

BIRD FRIENDLY GLAZING

As glazing becomes more prominent in buildings it has had a detrimental effect on birds. Confused by the reflective quality of glass, birds at greater and greater numbers are flying into buildings and dying. This effect is most readily observed in large skyscrapers since there may be more glazing present, but the largest impact is with buildings of two, three, and four stories because those are the most common heavily glazed building heights. In response to bird deaths, advocates and scientists have developed best practices for bird safe glazing which include glazing with embedded patterns and etching, and utilizing fins, and mullions to break up the reflective surfaces.



CODE RECOMMENDATIONS

DARK SKY NIGHT LIGHTNING

Light pollution is created by the widespread use of artificial light and has led to the brightening of the night sky over inhabited areas affecting wildlife, human health, and astronomy. On the other hand, lighting is critical to an active downtown and promoting safe streets. Therefore, to mitigate contributions to light pollution, the Village should investigate dark sky-approved products or dark sky-friendly lighting when specifying street lighting. Dark sky-friendly products direct light only where needed and reduce the amount of light that can contribute to skyglow. Generally, these light fixtures direct light down and are fully shielded. They can also be a “warmer” light temperature of about 3000-Kelvin as opposed to a “cooler” blue light which ranges around 5000-Kelvin and has a greater disruptive impact.



ARCHITECTURAL DESIGN GUIDELINES

APPROPRIATE BUILDING TYPES AND CHARACTER DEFINING ELEMENTS

Throughout the course of the Village Master Plan process and the public design charrette, the idea of architectural design guidelines has been discussed repeatedly. TCRPC has provided Design Guidelines for five different architectural styles deemed appropriate for the Village of Tequesta. Some of the architectural styles are more conducive to single family residential while some styles, like Mediterranean Revival and Mid-Century Modern are appropriate for single family and commercial or mixed-use buildings.

The intent of these guidelines is not to force or regulate these specific styles. The intent is to be inspirational and educational and for there to be a record of appropriate architectural styles for the Village. In addition, enough detail is provided as to the components that make up these styles they should assist in design review.



Excerpt of from the sample Architectural Design Guidelines found in Appendix D.

PRE-APPLICATION REQUIREMENTS

The Village has recently proposed updates to the redevelopment application process to require conceptual design meetings early in the process. This approach was discussed during the charrette process and is important to the Village being proactive in directing future development projects. Projects proposed within the Mixed-Use and C-2 districts need to have Village input and evaluation as early as possible within the development proposal process. This will improve efficiency, help applicants avoid unnecessary expense and delays, and improve outcomes.

DEVELOPMENT INCENTIVES

Many cities, towns, and villages consider the use of development incentives as a tool to ensure the local community's particular vision is being implemented. Often these will include increases in building height, density, intensity, parking reductions, or in the case of Community Redevelopment Areas (or special development areas) tax abatements or Tax Increment Financing offsets.

Another successful incentive is the potential for administrative approval of projects that meet the expectations of the community vision with little to no deviation. This approach is typically supported by very clear development regulations usually as a Form-Based Code. Some of those form-based elements are provided as recommendations in this plan. Also, while the potential for administrative approval with no public hearings may be very appealing to an applicant, it requires a good deal of trust between the community, the local government, and staff.

The concept of development incentives was raised during the Village of Tequesta planning process and there was little to no interest in raising building heights or increasing densities. Should the Village reconsider this concept at some point, regardless of the incentives being offered, it is recommended that the affected areas be limited and precise (i.e. Town Center District as identified on the Regulating Plan). There should be a clear and transparent process which specifically articulates the incentives offered and the public benefits gained.

PROMOTING LOCAL BUSINESSES

LOCAL BUSINESSES

One topic of discussion during the Tequesta charrette and planning process was the need/desire to regulate franchise or national retailers. There is an understandable concern that as new redevelopment is proposed in the Village, especially along the commercial corridors, there could be a loss of the local businesses that are prolific throughout the Village today. There are local governments that do regulate franchise businesses (the Town of Palm Beach Town-Serving Ordinance was referenced during the charrette) however their approaches differ and are typically tailored to the needs of each community.

There can be a risk to the over-regulation of, or outright prohibition of franchise businesses. The legal limits to the extent a local government can target specific businesses with additional regulations must be considered as well as the potential perception of not being businesses friendly or even elitist.

One approach is the Formula Business regulations developed by the City of Stuart, Florida provided at right. This approach defines a specific area of impact and limits formula businesses versus outright prohibiting them.

Sec. 2.06.12. Formula businesses.

- A. Procedure. Review of an application to locate a formula business within the formula business area described below shall follow the procedures set forth in section 3.01.06 for a major Urban Code conditional use.
- B. Formula business area. A formula business may be located in the area within the following boundaries only by major Urban Code conditional use. Commence at the northernmost point of Lot 10 Danforths Addition, thence along the St. Lucie River to Detroit Avenue, thence south along Detroit Avenue to East Ocean Boulevard, thence west along East Ocean Boulevard to the intersection with S.E. Flagler Street, thence south along S.E. Flagler Street to the easternmost point of Lot C of Lainhart and Potter addition and southwest across the F.E.C. Railroad right-of-way to S.E. Old Dixie Highway, thence southeast along S.E. Old Dixie Highway to Martin Luther King Jr. Boulevard, thence west along Martin Luther King Jr. Boulevard to the southerly extension of Camden Avenue, thence north along Camden Avenue to a point directly west of the northernmost point of lot 14, Block 15 Frazier Addition, thence west to the southerly extension of S.W. Albany Avenue, thence north along S.W. Albany Avenue to the F.E.C. Railroad right-of-way, thence across the F.E.C. Railroad right-of-way to the westernmost point of Lot 10 Danforths Addition, thence

PROMOTING LOCAL BUSINESSES

northeasterly along the west boundary of Lot 10 Danforths Addition to the point of beginning. The following graphic depicts this area.

- C. Standards. The following specific standards shall apply to formula businesses in the Urban Center, Urban Neighborhood, Urban General and the Urban Waterfront sub-districts of the Urban District.
 - (1) The proposed formula business will not alter the identity of the urban district in a way which detracts from its uniqueness or contributes to a nationwide trend of standardized downtown offerings.
 - (2) The proposed formula business will contribute to a diverse and appropriate blend of businesses in the urban district.
 - (3) The proposed formula business will complement those businesses already in the urban district and help promote and foster the local economic base as a whole.
 - (4) The proposed formula business will be compatible with existing surrounding uses.
 - (5) The proposed formula business has been designed and will be operated in a nonobtrusive manner to preserve the community's character and ambiance and the proposed intensity of uses on the site is appropriate given the uses permitted on the site and on adjoining sites.
 - (6) The proposed formula business meets all of the following fixed criteria:
 - a. The size of the business shall not exceed 1,500 square feet of gross floor area.
 - b. The first floor street frontage of the business shall not exceed 35 feet in width.
 - c. Corporate advertising shall not be used which is visible from the exterior of the premises.
 - d. Exterior facade colors shall comply with the requirements of the Urban Code.
 - e. Corporate structural elements and signage will be designed to be in harmony with architectural features

PROMOTING LOCAL BUSINESSES

and elements of the building as approved by the city development director.

- f. Signage colors shall be muted to minimize visual intrusion as approved by the city development director.
- g. No drive thru windows shall be used.
- D. Distance requirements. A formula business shall not locate within 300 feet of an existing formula business establishment.
- E. Existing formula business. A business may be a formula business that exists on the 14th day of February, 2005, the effective date of this ordinance. Any such business, and any business that becomes a formula business by virtue of the creation of six or more other such businesses, shall be deemed a legal nonconforming use of a building subject to the provisions of section 8.03.00 of this Code.



The map at left identifies the limited area of downtown Stuart where the Formula Business ordinance is in effect. Other ways to control the proliferation of franchise or formula businesses is to regulate the building types more stringently. Many of the formula businesses that communities seek to limit are often fast food restaurants with drive throughs. By prohibiting or restricting the configurations of drive throughs some of the community's concerns can be mitigated.

The Village should consider creating a Formula Business Overlay District restricting the number and type of national retail/restaurant chains. The overlay should be limited to the Village Center and Town Center Districts. There should be community outreach and discussion as part of this process.

VILLAGE OF TEQUESTA GOALS



III. KEY RECOMMENDATIONS & IMPLEMENTATION

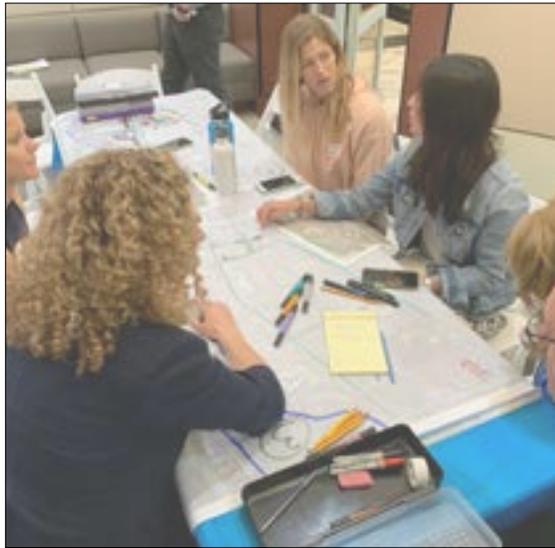
	PRIORITIZED ACTION ITEMS	ENTITIES FOR COORDINATION	TIME FRAME	DEPARTMENTS
A	Adopt the Commercial Corridors Master Plan - Start Public Information Campaign	Village of Tequesta	Six Months	Council
B	Begin discussions with Palm Beach County re: Old Dixie and Village Boulevard roundabout	Village of Tequesta, Palm Beach County, FDOT	Six Months - Year One	Admin., Public Works, Planning, Engineering
C	Explore canopy tree planting program for select roads in partnership with property owners with consideration of shared costs and maintenance	Village of Tequesta, Private Property Owners	Six Months - Year One (and annually)	Admin., Public Works, Planning
D	Begin Discussions with FDOT and Palm Beach County regarding resurfacing of N. Old Dixie Highway and the inclusion of a landscaped median during the scheduled improvements	Village of Tequesta, Palm Beach County, FDOT	Six Months - Year One	Admin., Public Works, Planning, Engineering
E	Consider Appropriate Development Incentives for Town Center and Main Street	Village of Tequesta, Property Owners	Year One	Admin., Planning
F	Begin updating Village Land Development Recommendations per Village Master Plan - codify District Centers	Village of Tequesta	Year One	Admin., Planning, Legal
G	Coordinate additional parkway improvements along the FEC Corridor north of Tequesta Drive	Village of Tequesta, FEC	Years One and Two	Admin., Public Works, Planning, Engineering
H	Adopt roadway designs (e.g., US Hwy 1, Old Dixie Highway, and Bridge Road)	Village of Tequesta, Palm Beach County, FDOT	Year One	Admin., Public Works, Planning, Engineering
I	Begin Discussion with FDOT re: Signalized Intersection at Village Boulevard and US-1.	Village of Tequesta, FDOT	Year One and Two	Admin., Public Works, Planning, Engineering

III. KEY RECOMMENDATIONS & IMPLEMENTATION

	PRIORITIZED ACTION ITEMS	ENTITIES FOR COORDINATION	TIME FRAME	DEPARTMENTS
J	Begin Discussions with CEMEX Corporation to explore opportunities to expand the FEC corridor linear park south of Tequesta Drive	Village of Tequesta, FEC, CEMEX Corporation	Year One and Two	Admin., Planning, Legal
K	Explore redevelopment options for the Fashion Mall - work with owners to build enthusiasm and forge a direction - coordinate with FDOT and utility providers	Village of Tequesta, Fashion Mall Owners, FDOT	Year One and Two	Admin., Planning, Legal
L	Consider creating a Formula Business Overlay District restricting the number and type of national retail/ restaurant chains. The overlay should be limited to the Village Center and Town Center Districts.	Village of Tequesta, community outreach	Year Two	Admin., Planning, Legal



APPENDIX A
CREATION OF THE MASTER PLAN



Citizen table at Charrette workshop.



Citizen table at Charrette workshop.



Work-in-progress presentation as part of Charrette.

OUTREACH

Public outreach and participation was an essential ingredient in the creation of this plan and recommendations. Many forms of outreach and opportunities were provided so that all who were interested in the process could participate.

HOST COMMITTEE

A Charrette Host Committee was established to assist the TCRPC team with logistics and outreach. The Host Committee also provided many of the contacts for the almost 30 individual interviews that were conducted.

CHARRETTE

A five-day public design charrette has held at the Village Council chambers from Saturday, April 9, 2022 to Wednesday, April 13, 2022. The public workshop was that Saturday. The Design team worked in the Council Chambers “studio” which was open to the public from 9:00 am until 9:00 pm each day.

WORK-IN-PROGRESS

A Work-in-Progress presentation was given the evening of Wednesday, April 13, 2022 and was the first opportunity for the public to see the design work and recommendations and provide their feedback and input.

TABLE 1



Citizen's presenting their workshop ideas

TABLE 2



TABLE 3

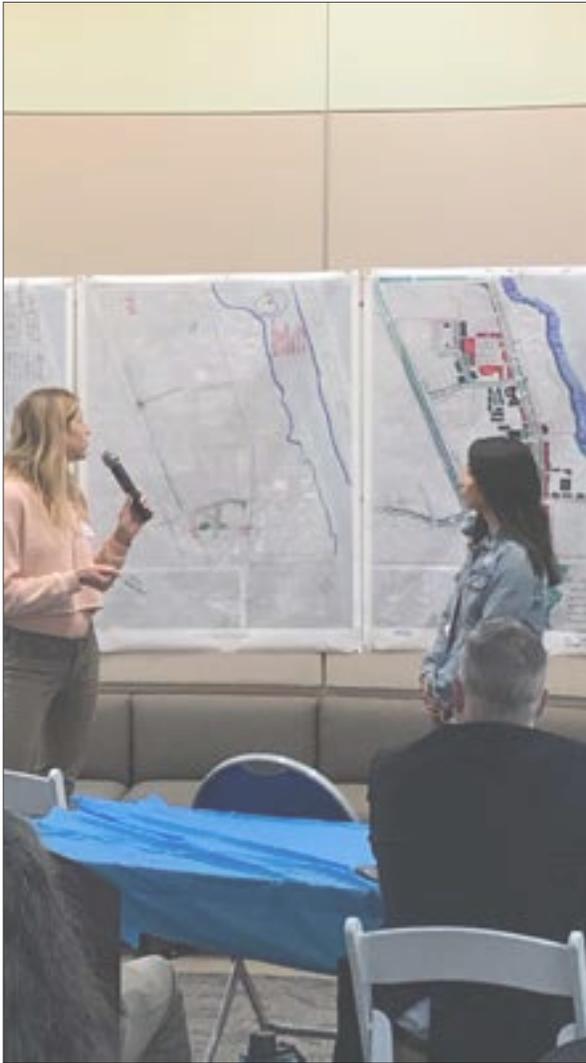


TABLE 4



Citizens presenting their workshop ideas

TABLE 5



Citizens presenting their workshop ideas

TABLE 1



TABLE 2



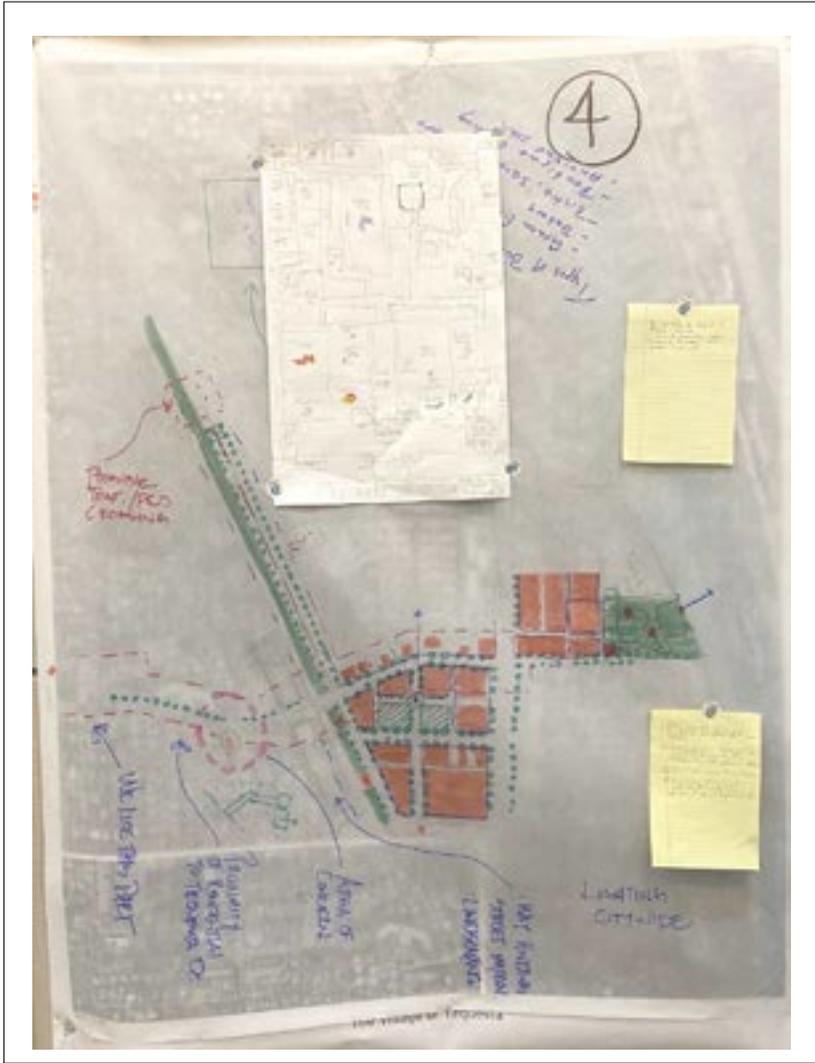
Citizen Charrette Plans

TABLE 3



Citizen Charrette Plans

TABLE 4



VILLAGE OF TEQUESTA 

COMMERCIAL CORRIDORS CHARRETTE

PUBLIC WORKSHOP
SATURDAY APRIL 9TH

Join the Village and the Treasure Coast Regional Planning Council in a public discussion on the desired future redevelopment of the commercial corridors in the Village of Tequesta.
LUNCH & REFRESHMENTS WILL BE PROVIDED

- **OPENING PRESENTATION: 10:00 AM**
- **PUBLIC DESIGN SESSION & LUNCH: 11:00 AM - 1:00 PM**
- **CITIZENS PRESENTATIONS: 1:00 PM - 2:30 PM**
- **WRAP UP: 3:00 PM**

LOCATION Village Hall Council Chambers
345 Tequesta Drive
Tequesta, Florida 33469

 For more information, please visit:
<https://www.tequesta.org/>

Charrette Flyer

PLEASE PROVIDE YOUR INPUT

The Treasure Coast Regional Planning Council, in collaboration with the Village of Tequesta, will conduct a public workshop and design charrette to analyze potential redevelopment strategies for commercial properties within the Village. The outcome of this effort is intended to provide a clear vision for residents and investors of desired redevelopment for commercial properties.





- **Saturday Workshop**
Saturday, April 9, 2022 10:00 am - 3:00 pm
- **Studio Day #1**
Sunday, April 10, 2022 9:00 am - 10:00 pm
- **Studio Day #2**
Monday, April 11, 2022 9:00 am - 10:00 pm
- **Studio Day #3**
Tuesday, April 12, 2022 9:00 am - 10:00 pm
- **Studio Day #4**
Wednesday, April 13, 2022 9:00 am - 2:00 pm
- **Work-In-Progress Presentation**
Wednesday, April 13, 2022 6:00 pm - 8:00 pm

The Saturday Public Workshop, Charrette Design Studio (open to the public), and the Work-In-Progress Presentation will all occur at the Village Hall Council Chambers.

For more information please contact:
Lori McWilliams, MMC, Village Clerk/Admin PID
561.768.0443, lmcwilliams@tequesta.org
Dana P. Little
Urban Design Director, Treasure Coast Regional Planning Council
772.221.4060, dlittle@tcrpc.org

APPENDIX B

BACKGROUND & EXISTING CONDITIONS

HISTORY

The Village of Tequesta was founded in the 1950's, incorporated in 1957, and was originally a planned community focused on the Tequesta Country Club, hence the Country Club neighborhood. Named after the Tequesta Indians who were indigenous to the area, Tequesta is the northernmost municipality in Palm Beach County and is adjacent to and south of Martin County. The Village is approximately 1.2 square miles of upland and as of 2022 has a population of 6,158. The Village is predominantly residential in use however it does have one mile of commercial frontage on US-1 and nearly 1/2 mile of commercial/industrial frontage on N. Old Dixie and S. Cypress Drive.

Enter Charlie Martyn

Shortly after he arrived in Palm Beach in 1955, real estate developer Charlie Martyn found the sequestered Jupiter inlet area. He was intrigued with the south end of the island (now the Jupiter Inlet Colony) and asked Mr. Cato, tender of the old turnstyle bridge to the island, to show him the area. Cato responded that "It is just a jungle" but took Martyn there in his boat. Thus began one of the more aggressive community developments in Florida.

Martyn bought 86 acres on the island, laid out 240 lots and a shopping area. (At that time there were just a few stores on U.S. 1 ... gas stations and restaurants.) The old Log Cabin restaurant was in Carlin Park and was off the old U.S. 1 southeast jog.



The entire Tequesta Police force in February of 1961 stands in front of the Tequesta developer's office on U.S. #1. It served as the Village administrative headquarters prior to the construction, a year and a half later, of the current Village Hall. Standing between the two radio-equipped cars are Sargent Petry, Chief Deuserbery, and John Roderick.

—Courtesy Village Files

1957....In The Beginning



Circa 1957's... Just prior to the development of the Country Club Community



Cato's bridge is named for one of the first bridge tenders. The wooden structure going to Jupiter Island was built in the 1920's and swung out to open for passing boats. The bridge was demolished in 1968 to make way for the today's existing bridge.

PREVIOUS MASTER PLAN

In 1989 the firms of Mark Schimmenti, Architects and Image Network (now Dover Kohl and Partners) were engaged by the Village to create a redevelopment master plan for the central part of Tequesta Drive between N. Old Dixie Highway and US-1. Part of the proposal was the redevelopment of a closed Winn Dixie shopping center. The result of that effort was the Village of Tequesta Town Center Master Plan (left).

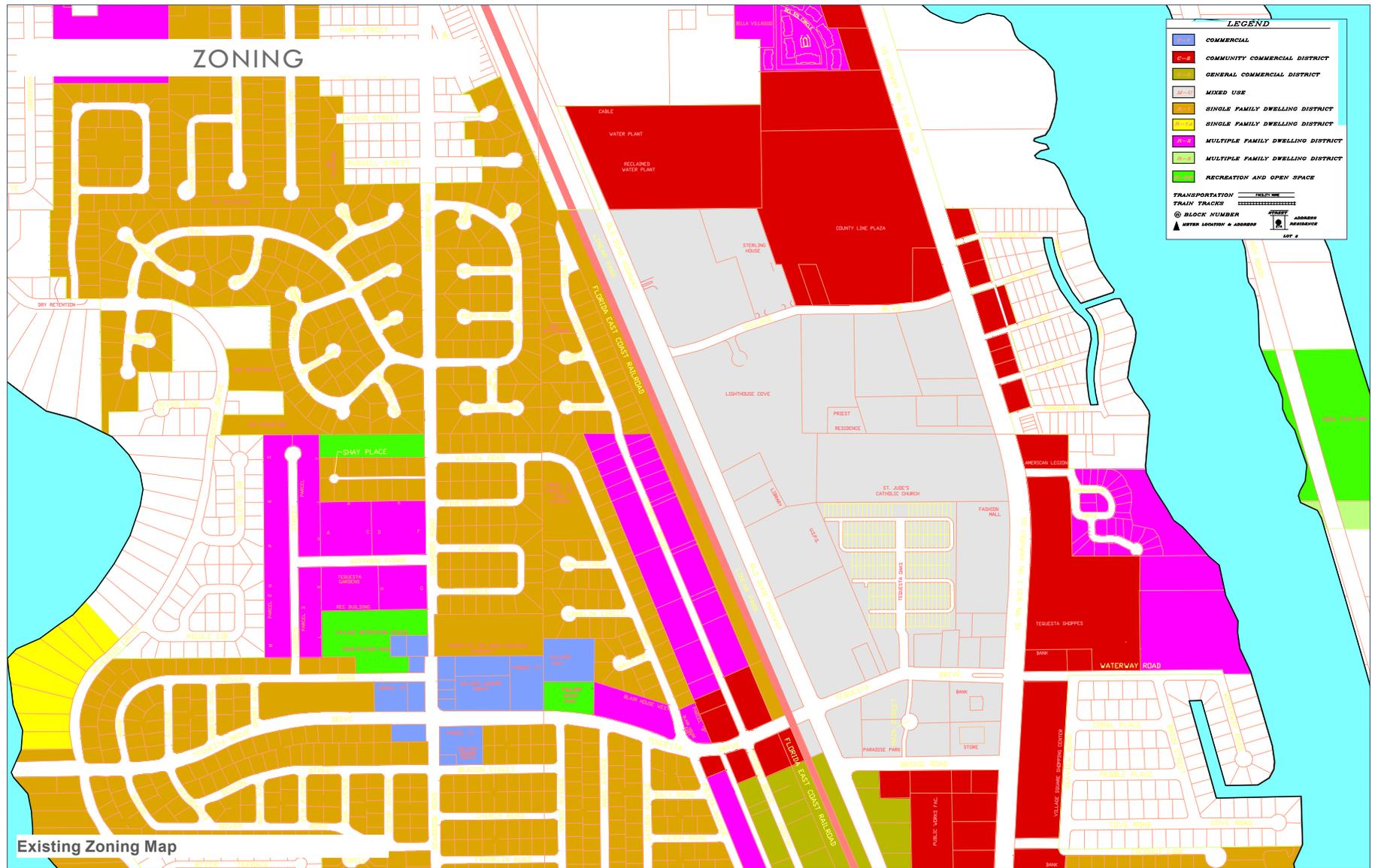
The Town Center plan engendered the creation of the Mixed-Use Zoning designation and part of the Town Center area that has been developed (below).



Diagram of existing Tequesta Master Plan



BACKGROUND & EXISTING CONDITIONS



A NEW MASTER PLAN

In 2021 the Village of Tequesta entered into an agreement with Treasure Coast Regional Planning Council (TCRPC) to conduct a public planning process and develop a master plan focusing on the commercial corridors in the Village. Increasing development pressures, in particular infill residential and mixed-use, were creating growing concerns within the community that there was not a clear vision for how new development



Village of Tequesta map

should look or be planned. The Village and TCRPC agreed that a five-day public design charrette, with preceding Host Committee meetings and stakeholder interviews, would be a good process to include the public and solicit their input and participation. The charrette was held from April 9, 2022 through April 13, 2022.

While the primary focus of the charrette and planning process was for the commercial corridors, other areas for improvement were identified. The village-scale and character was emphasized by the public as essential to remain and enhance. The Town Center, or Paradise Park, area is a critic nucleus of the Village and must be completed in an appropriate fashion. The local business environment, which is exceptionally strong in the Village of Tequesta must be protected and grown. In addition, specific recommendations for code revisions were requested as guidance for how the existing regulations should be modified or augmented to best ensure the community vision is implemented.

The purpose of this plan is to guide Village Council and staff, give direction to investors and developers, and provide a degree of certainty that the Village is growing in a manner acceptable to the community.



Silky the Porpoise



Tequesta (left) here. This aerial view from near Fairbank Point — circa 1950 — shows original rough grading of the Country Club community. In the background you see the North Fork of the Coconino River.

—Courtesy: Nancy Page

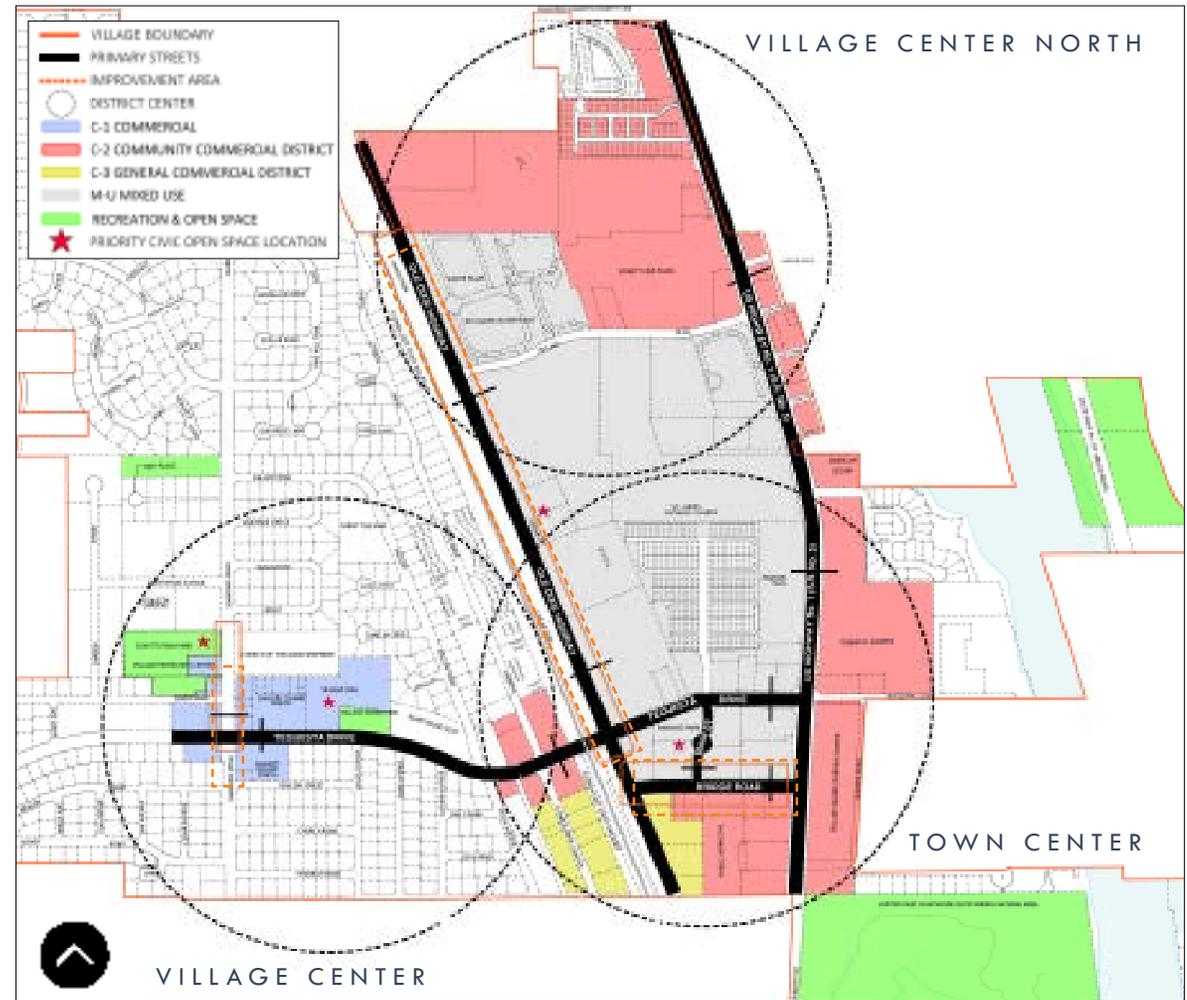
APPENDIX C
CODE RECOMMENDATIONS

TEQUESTA DISTRICT CENTERS

The Tequesta District Centers are identified as Village Center North (VCN), Town Center (TC), and Village Center (VC). The existing zoning for these centers are listed in the chart below. The primary roadway network is identified in black, and the priority locations for Civic Open Space is marked with a star. Code recommendations for these centers can be found on the following pages of this report.

Zoning District	Village Center (VC)	Town Center (TC)	Village Center North (VCN)
(C-1) Commercial	✓		
(C-2) Community Commercial	✓	✓	✓
(C-3) General Commercial		✓	
(M-U) Mixed-Use		✓	✓
Recreation and Open Space	✓		

FIGURE 04 TEQUESTA DISTRICT CENTERS - REGULATING PLAN



VILLAGE CENTER

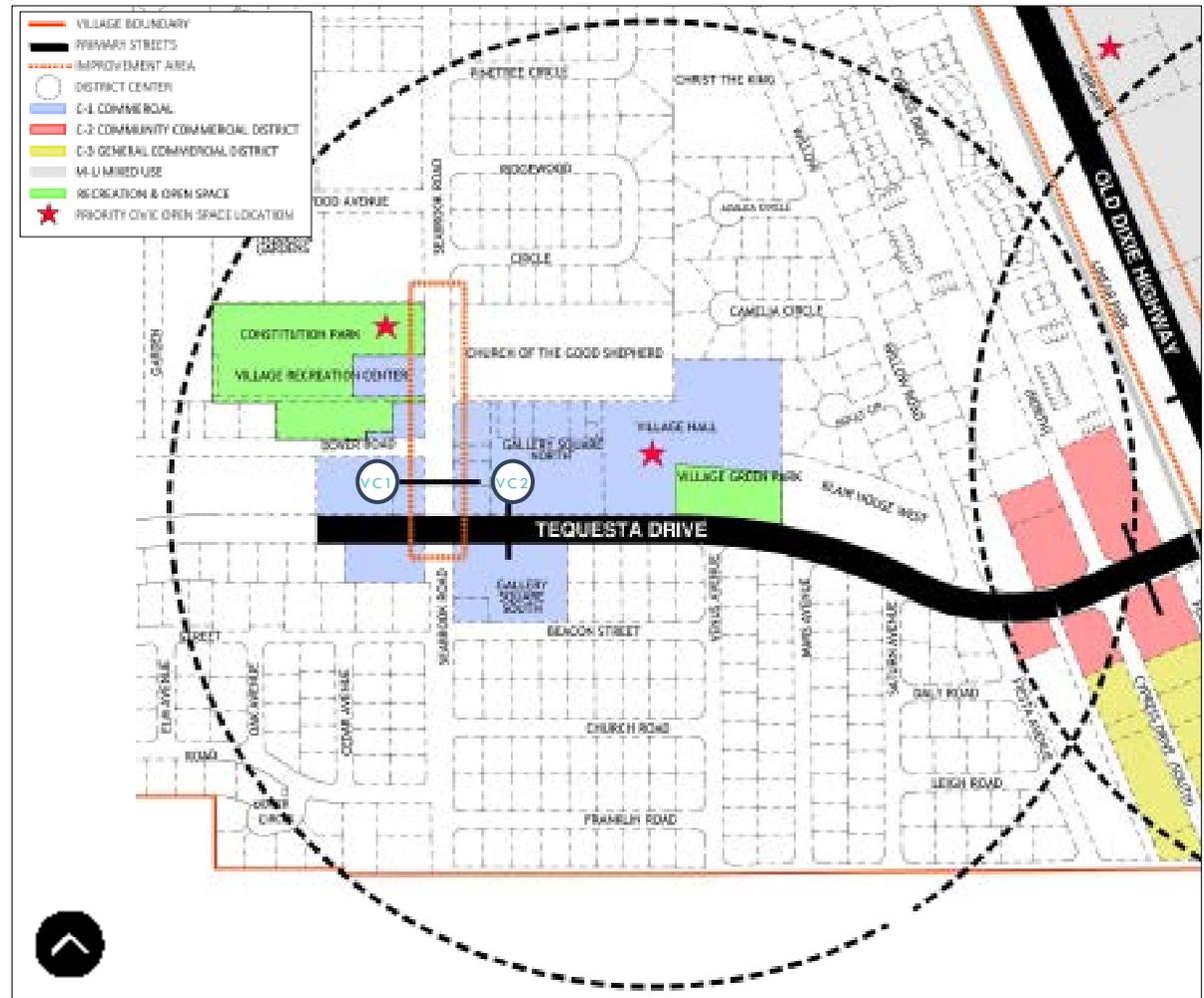
Description

The Village Center District (VC) is composed of the following zoning districts: M-U Mixed-Use (MU), Commercial (C-1), Community Commercial (C-2), and Recreation/Open Space Zoning Districts.

The Village Center District follows Tequesta Drive from the FEC corridor west to just beyond Seabrook Road. This 1/4 mile radius district is centered about the Gallery Square North Plaza and includes Gallery Square South, the Village Hall and Municipal Complex, the new Village Recreation Center, and Constitution Park.

This proposed district is intended to illustrate the unique neighborhood, resident, and local-business focus of the Village of Tequesta. To many in Tequesta this is the heart of the community. In making design and regulatory recommendations for this area it is important to distinguish this context and character from the Town Center District along Tequesta Drive east of the rail corridor.

FIGURE 05 VILLAGE CENTER REGULATING PLAN



VILLAGE CENTER

FIGURE 06 SEABROOK ROAD
- STREETScape

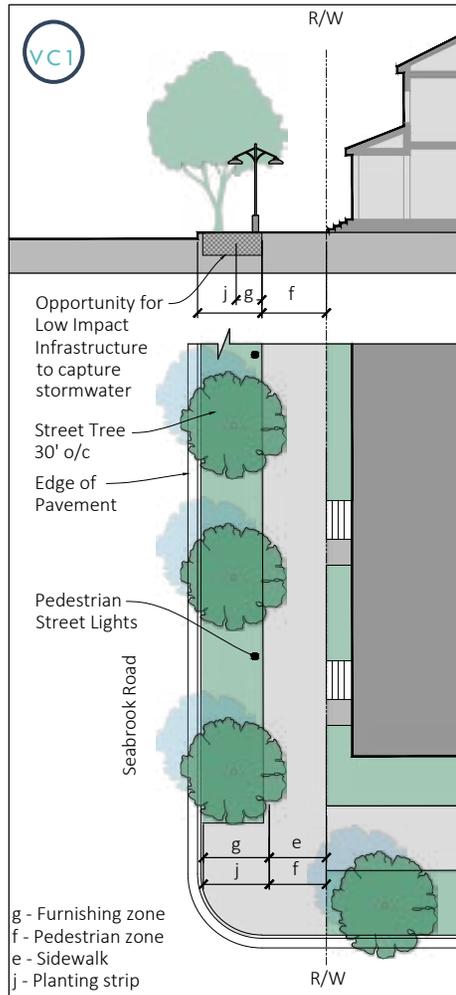
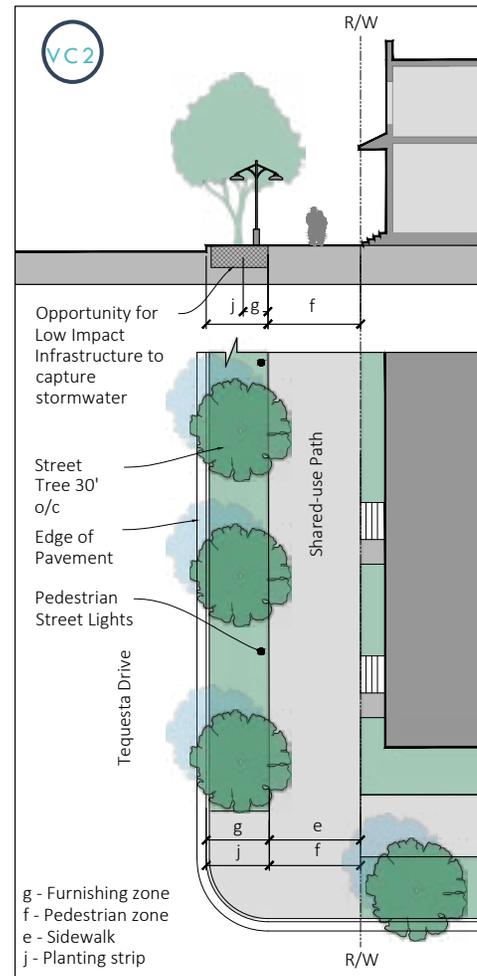


FIGURE 07 TEQUESTA DRIVE -
STREETScape

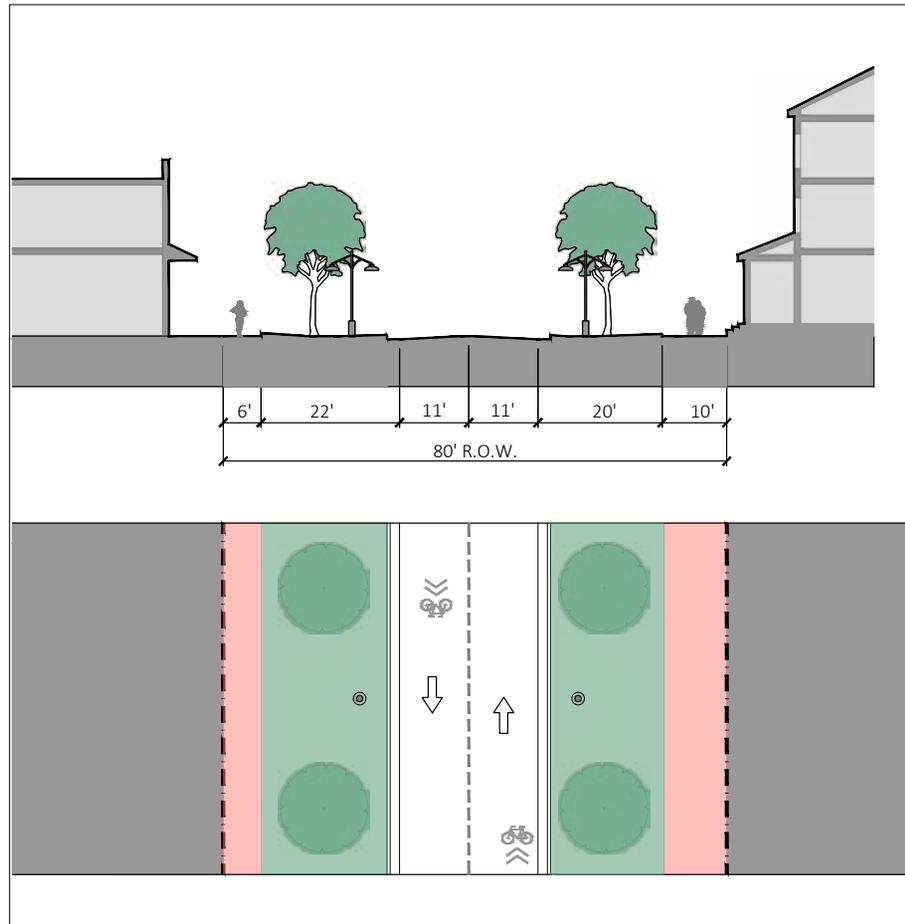


VILLAGE CENTER STREETScape

The two proposed Village Center Streetscapes sections (at left) are developed for Seabrook Road and Tequesta Drive. The primary function for each of these proposals is to incorporate wider sidewalks to accommodate pedestrians, cyclists, and where appropriate, sidewalk dining. Ample areas for shade tree plantings are important. In the case of Seabrook Road the approach is to improve the east side of the corridor to match the existing west side. Commercial and/or residential uses are anticipated for Tequesta Drive while primarily residential uses are proposed for Seabrook Road. This is reflected in the Frontage Types designations later in this chapter.

VILLAGE CENTER

FIGURE 08 SEABROOK ROAD IMPROVED



SEABROOK ROAD IMPROVEMENTS

This proposed street section for Seabrook Road is looking north near Gallery Square North (right hand side). This diagram illustrates Gallery Square North redeveloped with residential uses facing the street and continuous sidewalks. The improved east side (right side) of the roadway is intended to match the existing west side and together create a beautiful walking and biking environment connecting Tequesta Drive to the new Recreation Center.

Type	
Vehicular travel lanes	11 feet
Bike Facility	Shared-use path
R.O.W.	80 feet
Pavement Width	11 feet & 11 feet
Sidewalk	16 feet
Road Edge Treatment	Curb and gutter
Planting	Shade trees 30 feet on center occur in the landscape strip
Low Impact Infrastructure	Bioswale integrated landscape strips

TOWN CENTER

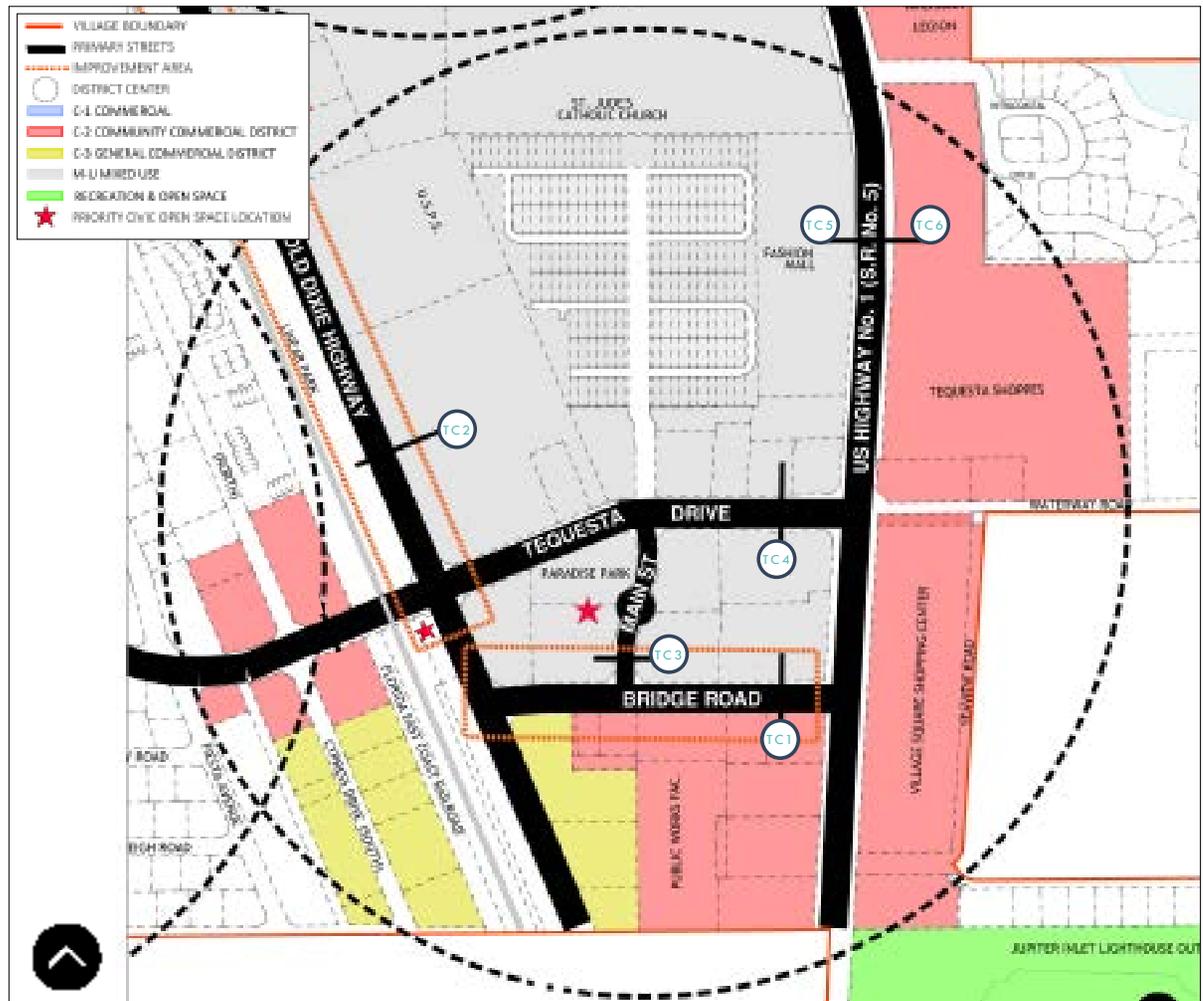
Description

The Town Center District (TC) is composed of the following zoning districts: M-U Mixed-Use (MU), Community Commercial (C-2), and Community General (C-3) Zoning Districts.

The Town Center District follows Tequesta Drive from the FEC corridor east to just beyond the Tequesta Shoppes shopping plaza. This 1/4 mile radius district is centered about the intersection of Tequesta Drive and Main Street and includes the entire Paradise Park/Town Center area, all of the Bridge Road corridor, Cypress Drive South, Village Square Shopping Center, and the Fashion Mall fronting US-1.

This proposed district is intended to embody its name, Town Center; a place where locals and visitors feel comfortable shopping, dining, and recreating. Establishing a comfortable, safe, and beautiful pedestrian environment framed by more urban buildings in this area is essential to achieving the vision. Completing the Town Center at Paradise Park consistent with the vision of this plan is vital to this entire district.

FIGURE 09 TOWN CENTER REGULATING PLAN



TOWN CENTER

FIGURE 10 BRIDGE ROAD - STREETSCAPE

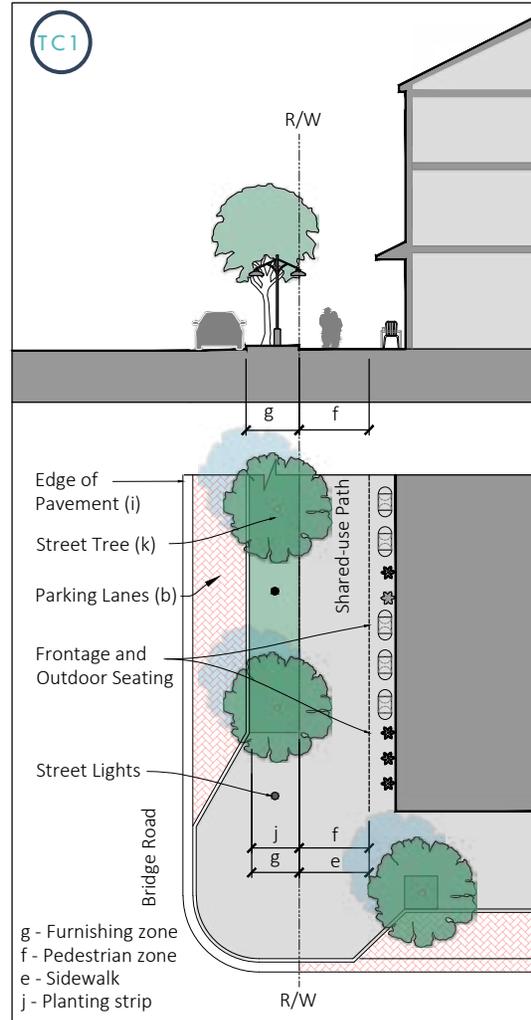
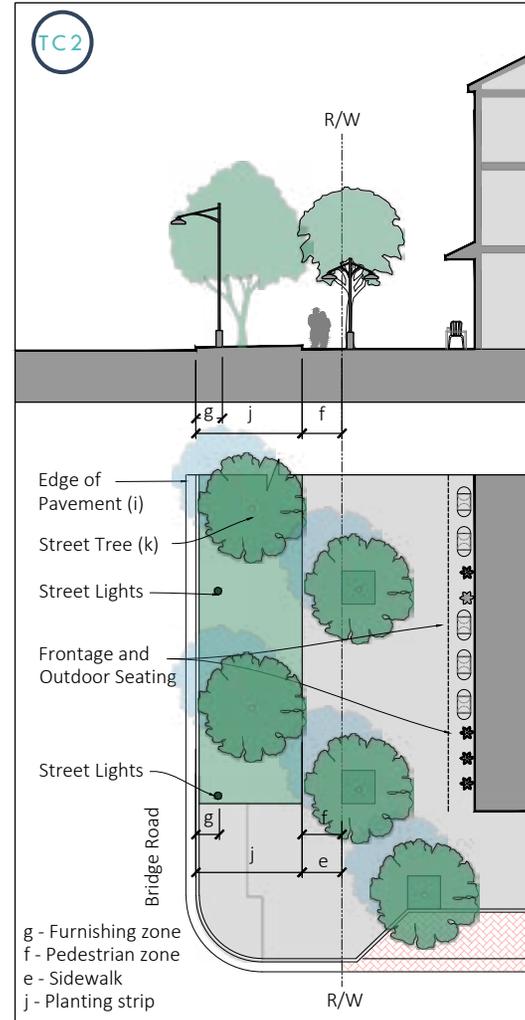


FIGURE 11 N. OLD DIXIE HIGHWAY - STREETSCAPE



TOWN CENTER STREETScape

There are six different street section proposals for the Town Center District: Bridge Road, N. Old Dixie, Main Street, Tequesta Drive East, and US-1 northbound and southbound.

Each one has very similar characteristics including ample sidewalks, shade trees, and clear zones for different activities or functions within the sidewalks.

The Bridge Road streetscape (far left) incorporates on-street parking to compliment the local business activity on the south side of the roadway.

The N. Old Dixie streetscape (left) includes very wide sidewalks and wide planting strips to accommodate large shade trees.

The location of each of these sections can be found on page C-6 as "TC1" and "TC2".

TOWN CENTER

FIGURE 12 MAIN STREET - STREETSCAPE

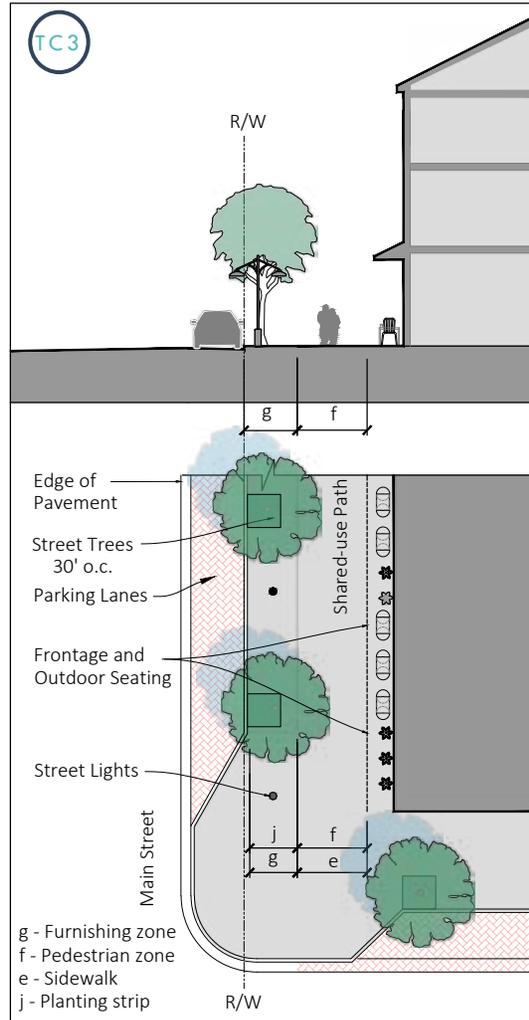
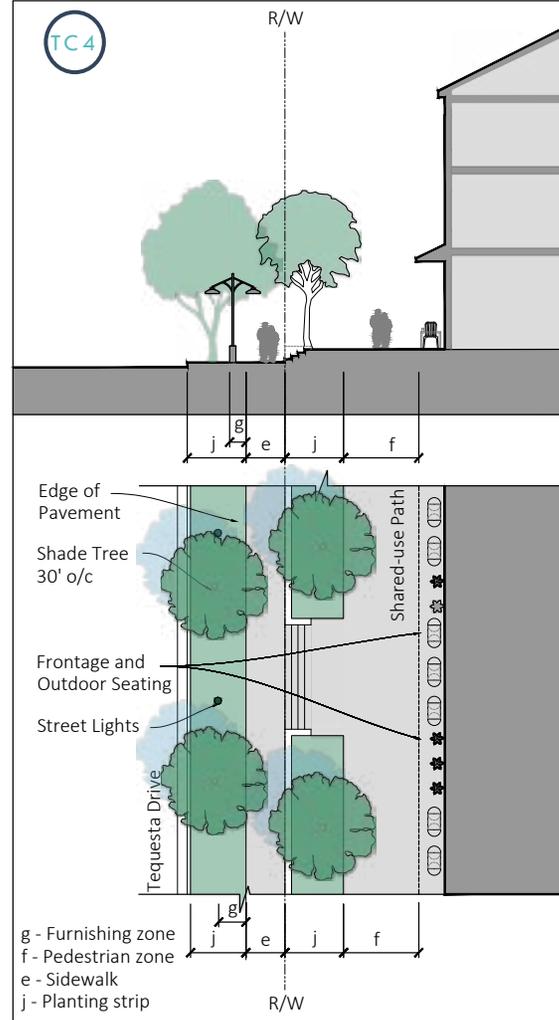


FIGURE 13 TEQUESTA DRIVE EAST - STREETSCAPE



TOWN CENTER STREETSCAPE

The Main Street street section (far left) proposes wide, multi-use sidewalks, shade trees in grates, and on-street parking. On-street parking, particularly in the Town Center and Bridge Road are necessary to help activate the street and provide a protective barrier to pedestrians and outdoor diners.

Tequesta Drive East (left) includes a unique condition where there is an existing change in grade on the south side at the old Bank of America site. This section incorporates that grade change to create a raised plaza treatment for future redevelopment at the corner which is unique to Tequesta.

The location of each of these sections can be found on page C-6 as "TC3" and "TC4".

TOWN CENTER

FIGURE 15 U.S. HIGHWAY 1 (WEST SIDE AT FASHION MALL) - STREETSCAPE

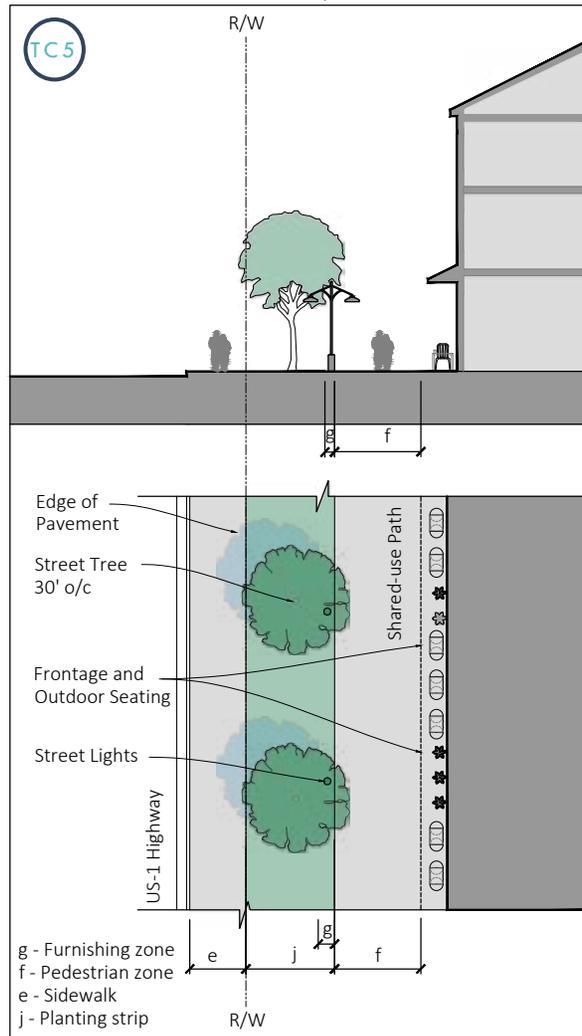
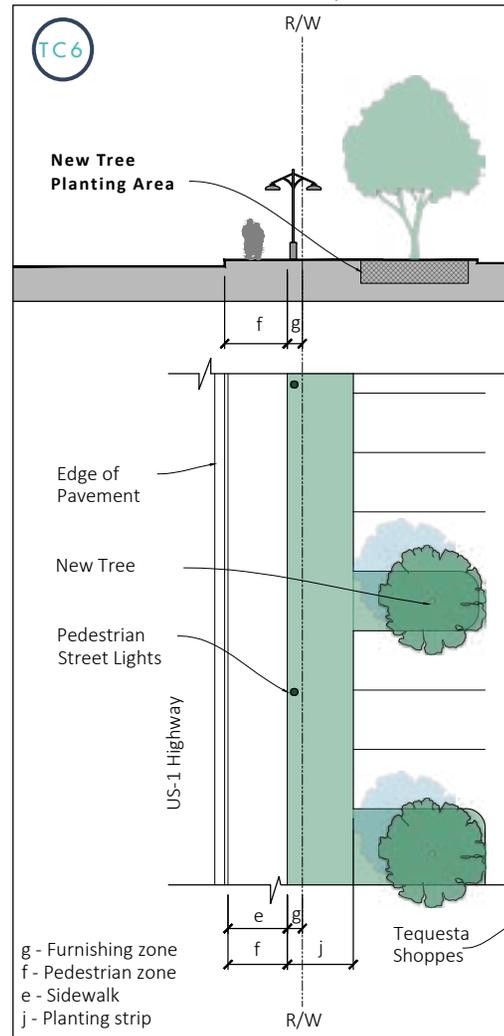


FIGURE 14 U.S. HIGHWAY 1 (EAST SIDE AT TEQUESTA SHOPPES) - STREETSCAPE



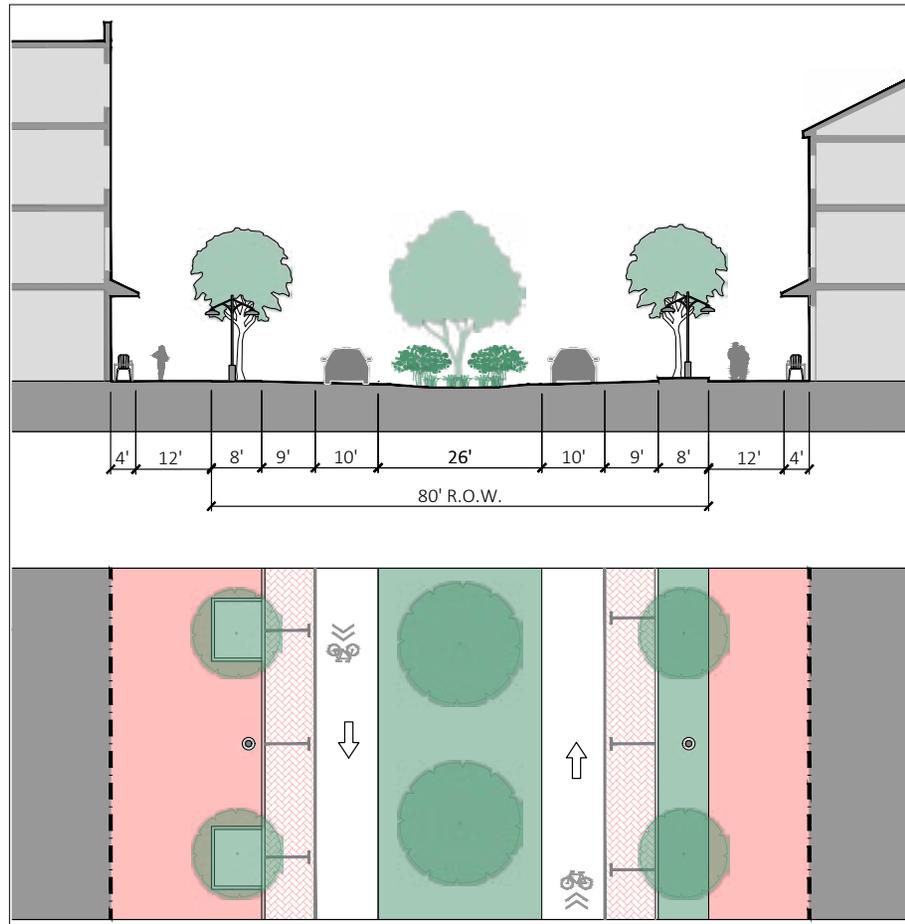
TOWN CENTER STREETSCAPE

The US-1 street section proposals are located in the same area along the corridor with one looking southbound (far left) and one looking northbound (left). The southbound section is cut through the right-of-way in front of a redeveloped Fashion Mall. Here the existing sidewalk remains, a parkway is added to provide shade trees, and a very wide sidewalk/multi-purpose pathway proposed. In this location the sidewalk/multi-purpose path should be at least 20' wide.

The northbound section (left) is cut through the right-of-way in front of Tequesta Shoppes and illustrates the incremental infill of shade trees on the private property. This proposal recommends the periodic removal of existing parking spaces to incorporate new tree planting areas large enough to accommodate large shade trees. This approach would be part of a collaborative public-private shade tree program.

TOWN CENTER

FIGURE 16 BRIDGE ROAD IMPROVED



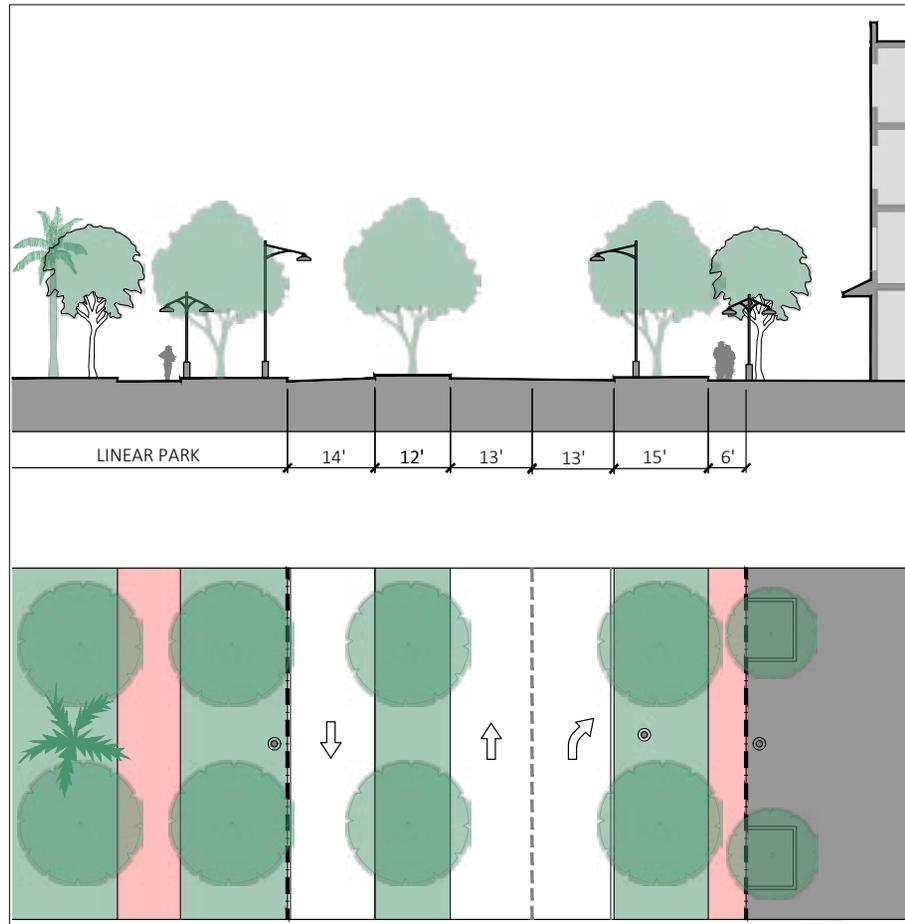
BRIDGE ROAD IMPROVEMENT AREA

The Bridge Road street section (left) is looking east with the existing and proposed businesses to the right (south) and the redeveloped Town Center to the left (north). Bridge Road is proposed to have a wide bioswale in the middle as a beautiful planting area that also functions as storm water drainage. Continuous on-street parking with sharrows and shared-use paths are also included.

Type	
Vehicular travel lanes	10 feet
Parking Lanes	9 feet parallel on both sides
Bike Facility	Sharrows & Shared-use path
R.O.W.	80 feet
Pavement Width	19 feet & 19 feet
Sidewalk	24 feet
Median	26 feet
Road Edge Treatment	Valley gutter
Planting	Shade trees 30 feet on center, may occur in the tree wells or landscape strip within the furnishing zone or within the parking lanes.
Low Impact Infrastructure	Bioswale integrated median, permeable surface at on-street parking

TOWN CENTER

FIGURE 17 N. OLD DIXIE HIGHWAY IMPROVED



N. OLD DIXIE HIGHWAY IMPROVEMENTS

The N. Old Dixie Highway section (left) is looking north and illustrates redevelopment to the right (east) and the FEC corridor to the left (west). This section proposes a new landscaped median where there is currently a painted-out, non-travel lane in the middle of the corridor. The creation of the median would accommodate shade trees.

Type	
Vehicular travel lanes	13-14 feet
Parking Lanes	N/A
Bike Facility	Shared-use path (Linear Park)
R.O.W.	73 feet
Pavement Width	14 feet & 26 feet
Sidewalk	16 feet
Median	12 feet
Road Edge Treatment	Curb and gutter
Planting	Shade trees 30 feet on center, may occur in the tree wells or landscape strip within the furnishing zone
Low Impact Infrastructure	Dark Sky night lighting

VILLAGE CENTER NORTH

Description

The Village Center North District (VCN) is composed of the following zoning districts: M-U Mixed-Use (MU) and Community Commercial (C-2) Districts.

The Village Center North District encompasses the northern end of the commercial corridors in Tequesta and stretches from Old Dixie Highway east to beyond US-1. This district is also defined by a 5 minute walk radius (1/4 mile) which includes County Line Plaza, County Line Road, the Water Treatment Plant, and Village Boulevard.

The character and context of this district is much different and more suburban in its development pattern than the Village Center and Town Center districts. Proposed regulations might be more flexible and focus more on roadway and pedestrian mobility improvements rather than actual redevelopment.

FIGURE 18 VILLAGE CENTER NORTH REGULATING PLAN



VILLAGE CENTER NORTH

FIGURE 19 N. OLD DIXIE HIGHWAY - STREETScape

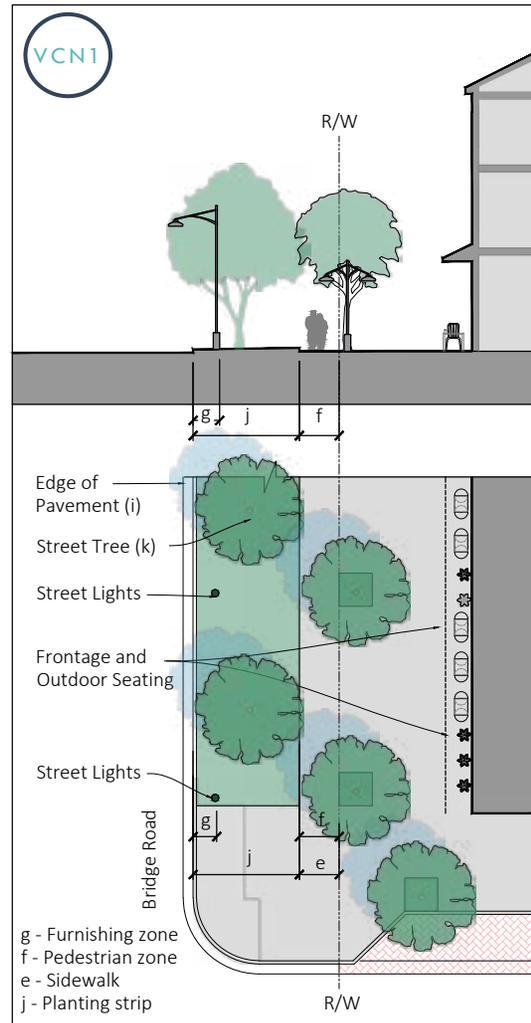
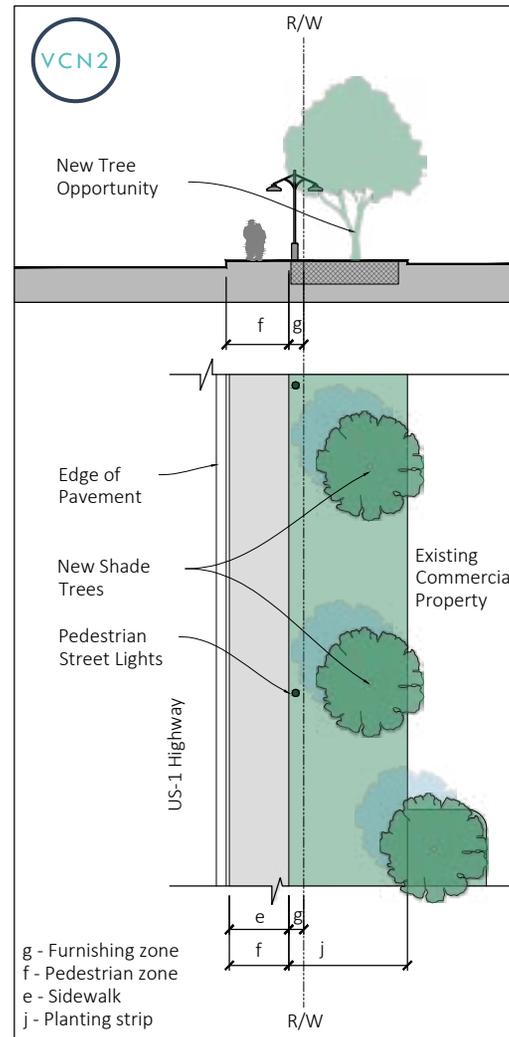


FIGURE 20 U.S. HIGHWAY 1 - STREETScape



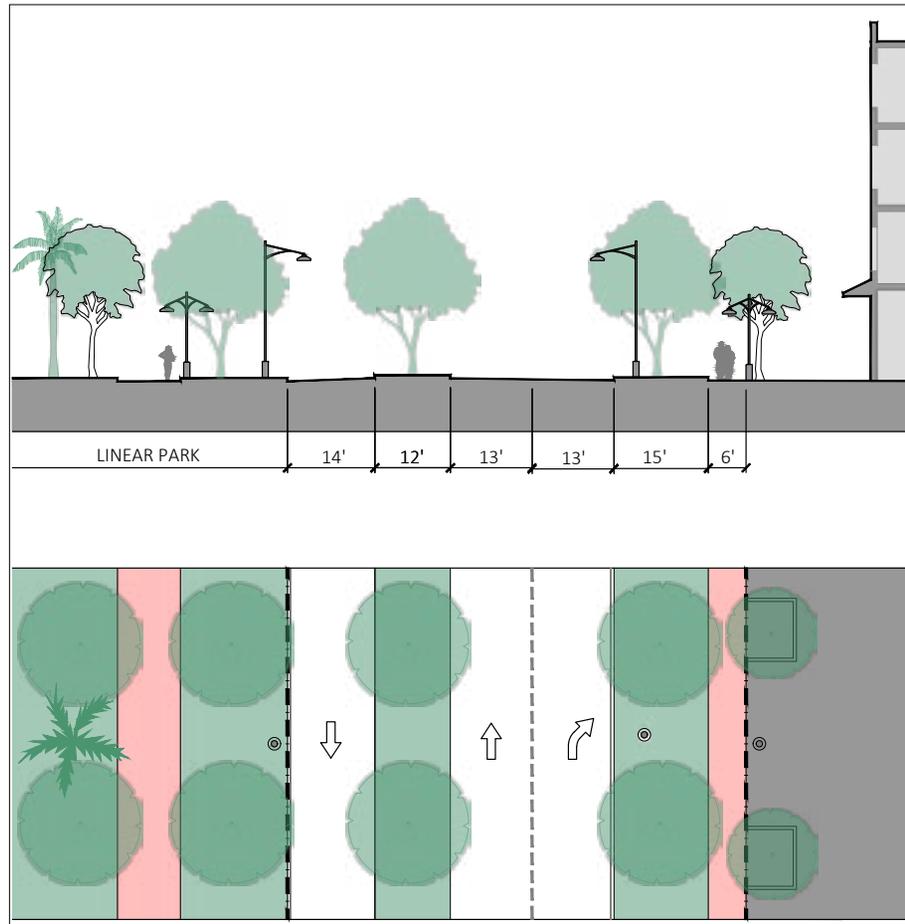
VILLAGE CENTER NORTH STREETScape

N. Old Dixie (far left) section includes a continuous 6' sidewalk and a planting strip of a minimum of 15' wide to provide large shade trees. The section also illustrates redevelopment on the east side of the corridor that might occur in other locations. This theme should be as consistent as possible along the corridor.

US 1 streetscape (left) is a strategy to provide more shade trees along the corridor on private commercial property. This approach is consistent with the section provided in front of the Tequesta Shoppes further south. Incentives should be offered to the property owners to provide shade trees in existing landscape areas adjacent to the sidewalk. This approach could be part of the public-private shade tree program.

VILLAGE CENTER NORTH

FIGURE 21 N. OLD DIXIE HIGHWAY IMPROVED



N. OLD DIXIE HIGHWAY IMPROVEMENTS

This section of N. Old Dixie illustrates the entire width of the street including enhancements to the linear park. In addition to the provision of additional shade trees, this proposal focuses on the creation of a new landscaped median in those areas where the center of the roadway is painted-out. This should be coordinated with any planned FDOT improvements.

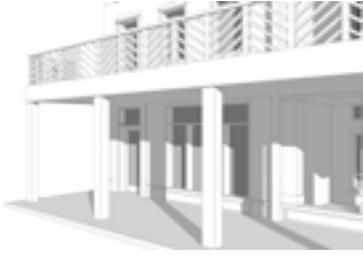
Type	
Vehicular travel lanes	13-14 feet
Parking Lanes	N/A
Bike Facility	Shared-use path (Linear Park)
R.O.W.	73 feet
Pavement Width	14 feet & 26 feet
Sidewalk	16 feet
Median	12 feet
Road Edge Treatment	Curb and gutter
Planting	Shade trees 30 feet on center, may occur in the tree wells or landscape strip within the furnishing zone
Low Impact Infrastructure	Dark Sky night lighting

FRONTAGE STANDARDS

FRONTAGE TYPES

This compendium of building Frontages Types are assigned to Primary Streets identified in the different districts. Not all frontages are appropriate for all streets. As an example, a storefront frontage type would not be appropriate on a strictly residential street just as a porch type would not be appropriate on US-1. By controlling the Frontages Types, along with the suggested street section changes, the public realm of Tequesta should be well protected as redevelopment occurs. The following pages recommend Frontage Types per district.

FIGURE 22 FRONTAGE TYPES MATRIX

		
<p>PORCH</p>	<p>STOOP</p>	<p>BRACKETED BALCONY</p>
		
<p>FORECOURT</p>	<p>ARCADE</p>	<p>SHOPFRONT</p>

FRONTAGE STANDARDS

VILLAGE CENTER FRONTAGE TYPES

As described earlier the Village Center District is more neighborhood and residential-oriented than the other districts, therefore there are a limited number of appropriate Frontage Types. Due to the neighborhood-serving businesses on Tequesta Drive the Storefront type would be appropriate however, that would not be the case for Seabrook Road. The table below assigns Frontage Types per street.



FIGURE 23 PERMITTED FRONTAGE TYPES MATRIX

VILLAGE CENTER		
Permitted Frontage Types	Tequesta Dr. West	Seabrook Road
Porch	✓	✓
Stoop	✓	✓
Bracketed Balcony	✗	✗
Forecourt	✓	✓
Arcade	✗	✗
Shopfront	✓	✗

FRONTAGE STANDARDS

TOWN CENTER FRONTAGE TYPES

The Town Center District is intended to be a destination for residents and visitors from outside of Tequesta. The table below indicates appropriate Frontage Types per Primary street. Note the limited application of the Porch and Arcade types. Bridge Road, while part of the Town Center/Paradise Park area is unique in that it could easily be a strictly commercial, strictly residential, or mixed-use street. The Porch Frontage type is associated with single family homes so its applicability in the Town Center is very limited.

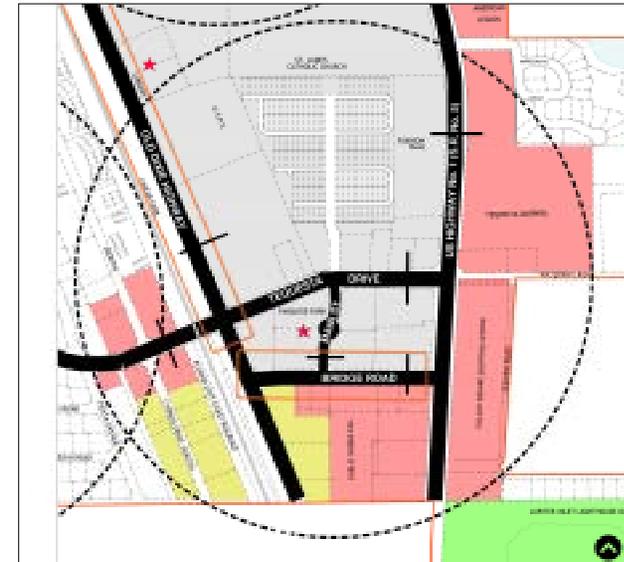


FIGURE 24 PERMITTED FRONTAGE TYPES MATRIX

TOWN CENTER							
Permitted Frontage Types	N. Old Dixie Hwy.	U.S. Hwy 1	Tequesta Dr. East	Tequesta Dr. West	Bridge Road	Cypress Dr. South	Main Street
Porch	✗	✗	✗	✗	✗	✗	✗
Stoop	✓	✓	✓	✗	✓	✗	✗
Bracketed Balcony	✓	✓	✓	✓	✓	✗	✗
Forecourt	✓	✓	✓	✓	✓	✓	✓
Arcade	✗	✗	✓	✗	✓	✗	✓
Shopfront	✓	✓	✓	✓	✓	✓	✓

FRONTAGE STANDARDS

VILLAGE CENTER NORTH FRONTAGE TYPES

As described earlier the Village Center North District is more suburban in its development pattern and has limited opportunities for infill redevelopment. The large commercial parcels are not likely to redevelop soon and the majority of the N. Old Dixie frontage (east side) is built-out with residential. This is the reason that the majority of recommendations for this district are landscaping and shade trees. In the event however that infill redevelopment does occur the appropriate Frontage Types per street are listed below.

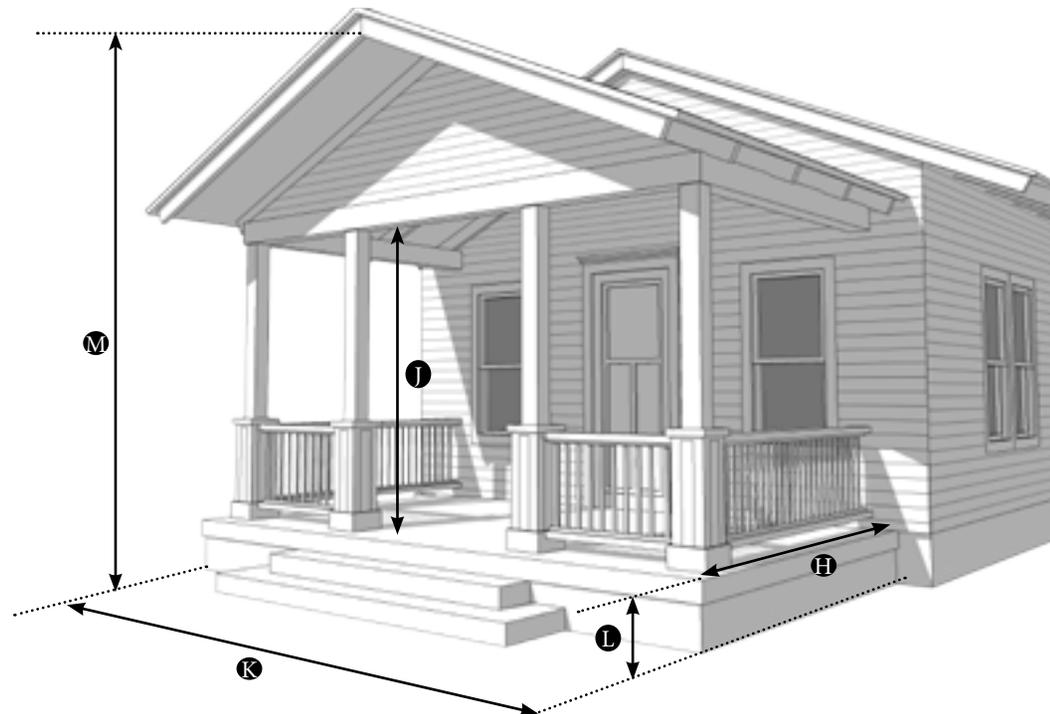


FIGURE 25 PERMITTED FRONTAGE TYPES MATRIX

VILLAGE CENTER NORTH			
Permitted Frontage Types	N. Old Dixie Hwy.	U.S. Hwy 1	Village Blvd.
Porch	✗	✗	✓
Stoop	✓	✓	✓
Bracketed Balcony	✓	✓	✓
Forecourt	✓	✓	✓
Arcade	✗	✗	✗
Shopfront	✗	✓	✗

FRONTAGE STANDARDS

FIGURE 26 PORCH FRONTAGE



Description

A porch is an open-air structure attached to a building forming a covered entrance large enough for comfortable use as an outdoor room. Front porches may be screened.

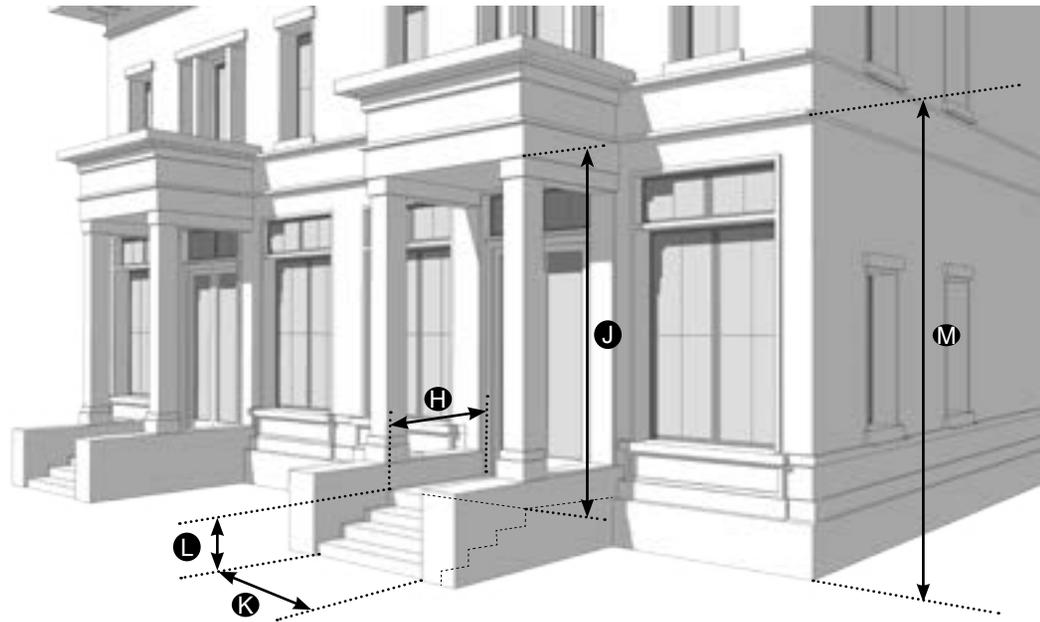
Dimensions

Depth	6 feet min. 8 feet preferred	H
Height, clear	8 feet min.	J
Width, length of facade	40% min.	K
Finish level above finished grade	21 inches min.	L
Height, stories	2 stories max.	M
Set back from curb	Not applicable	-



FRONTAGE STANDARDS

FIGURE 27 STOOP FRONTAGE



Description

A stoop is a small staircase leading to the entrance of a building that may be covered. The elevation of the stoop is necessary to ensure privacy for residential uses in the ground story of buildings. Stoops should provide sufficient space for a person to comfortably pause before entering or after exiting the building.

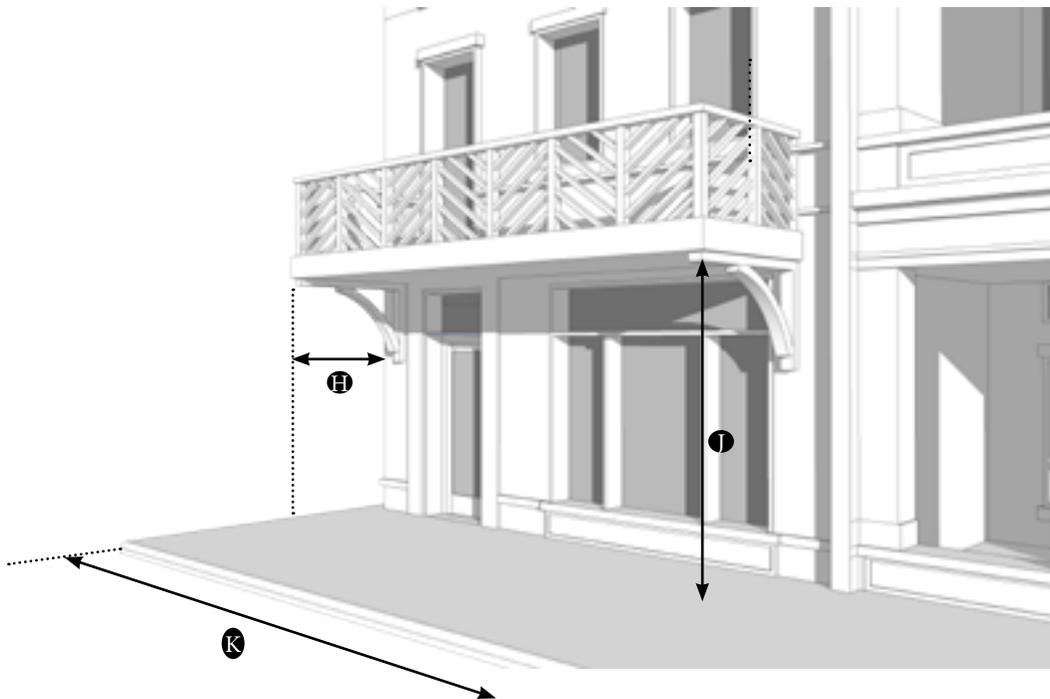
Dimensions

Depth	5 feet min.	H
Height, clear	8 feet min.	J
Width, clear	4 feet min.	K
Finish level above finished grade	21 inches min.	L
Height, stories	1 story max.	M
Set back from curb	Not applicable	-



FRONTAGE STANDARDS

FIGURE 28 BRACKETED BALCONY FRONTAGE



Description

A bracketed balcony is a second-story balcony, that creates a semi-public space overlooking the street above a main entry or unit. Bracketed balconies are typically associated with buildings with commercial uses in the ground story; however, bracketed balconies may be used with residential uses and in combination with a storefront or a stoop.

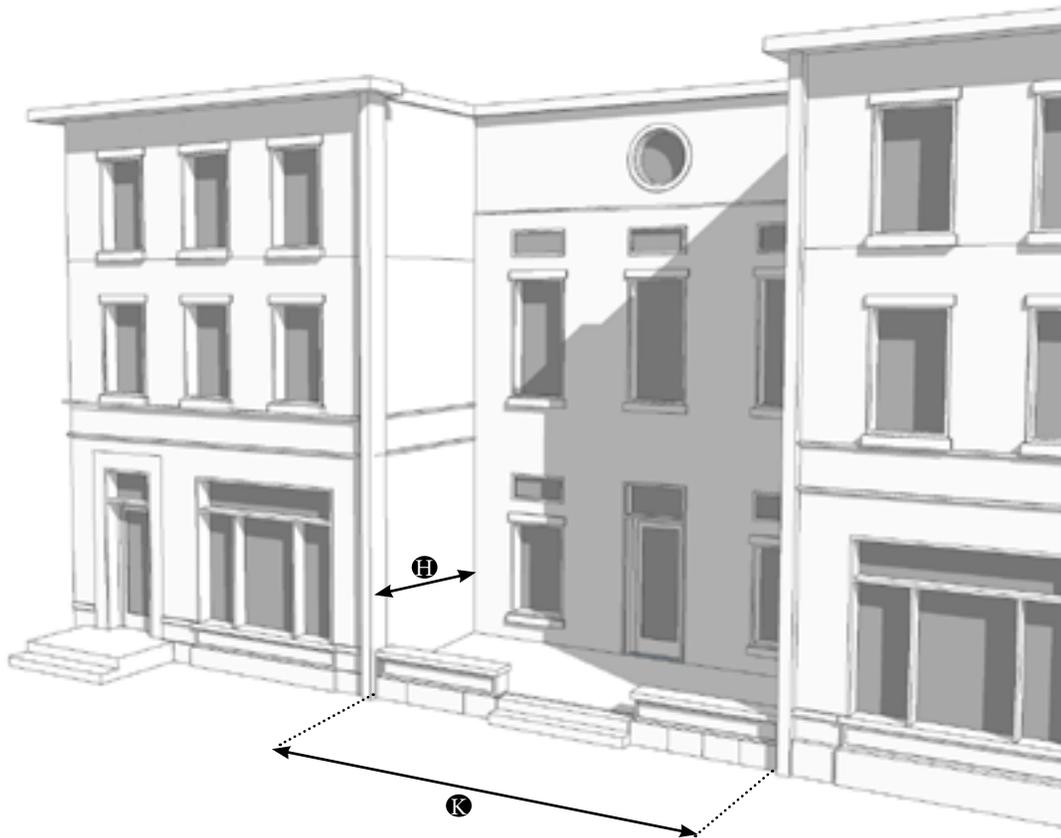
Dimensions

Depth	5 feet max.	H
Height, ground level clear	10 feet min.	J
Width	4 feet min.	K
Finish level above finished grade	Not applicable	-
Height, stories	Not applicable	-
Set back from curb	Not applicable	-



FRONTAGE STANDARDS

FIGURE 29 FORECOURT FRONTAGE



Description

A forecourt is an open area in front of the main building entrance(s) designed as a small garden or plaza. Low walls or balustrades no higher than three feet six inches in height when solid may enclose the forecourt. Forecourt walls are constructed of similar material as the principal building or are composed of a continuous, maintained hedge. A forecourt may afford access to one or more first floor residential dwelling units or incorporate storefronts for commercial uses. Forecourts are typically associated with multifamily, mixed-use, and commercial buildings.

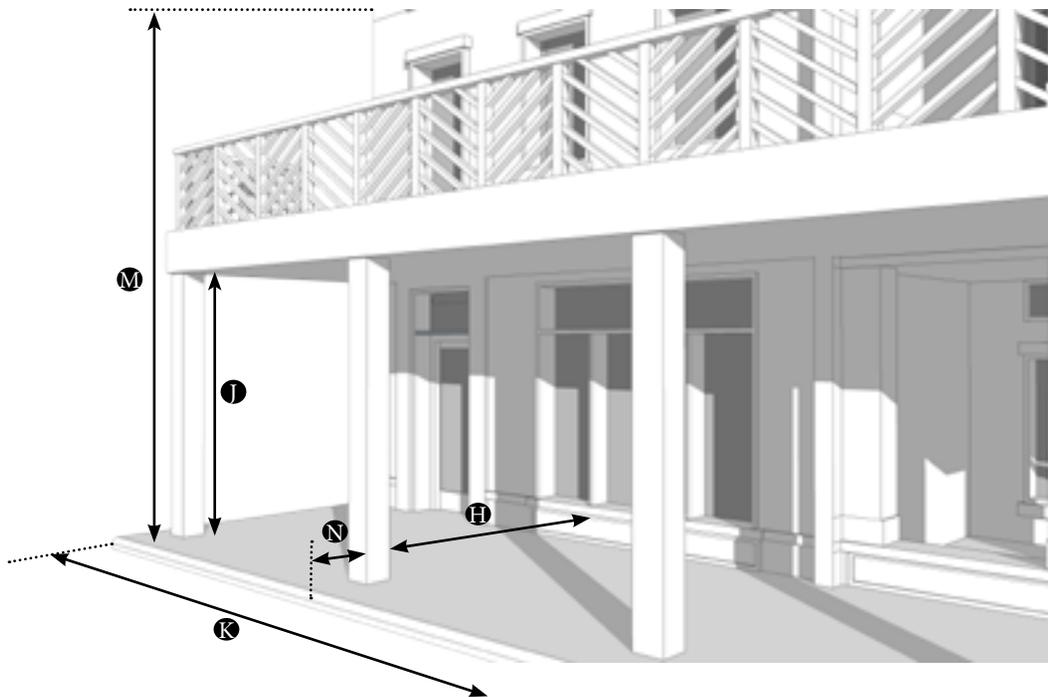
Dimensions

Depth, clear	20 feet max.	H
Height, clear	Not required	-
Width, length of facade	12 feet min. / 50% of facade max.	K
Finish level above finished grade	Not required	-



FRONTAGE STANDARDS

FIGURE 30 ARCADE FRONTAGE



Description

An arcade is a covered, unglazed, linear hallway attached to the front of a building, supported by columns or pillars. The arcade extends into the public right-of-way, over the streetscape area, creating a shaded environment ideal for pedestrians. This frontage type is typically associated with commercial uses. Arcades shall remain open to the public at all times. In the case where an arcade encroaches into the public right-of-way, a right-of-way maintenance agreement may be required.

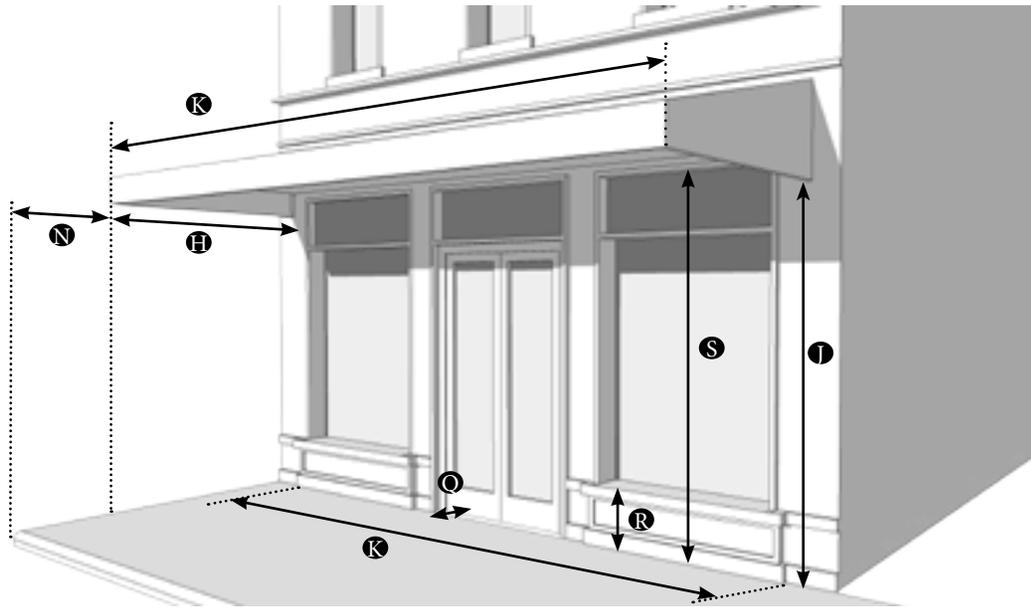
Dimensions

Depth, clear	8 feet min.	H
Height, ground level clear	10 feet min.	J
Width, length of facade	70% min.	K
Finish level above finished grade	at sidewalk level	-
Height, stories	2 stories max.	M
Set back from curb	2 feet min. / 4 feet max.	N



FRONTAGE STANDARDS

FIGURE 31 SHOPFRONT FRONTAGE



Description

The shopfront is a frontage type along the sidewalk level of the ground story, typically associated with commercial uses. Shopfront are frequently shaded by awnings or arcades.

Dimensions

Width, length of facade	70% min.	K
Door recess	10 feet max.	C
Storefront base	1 foot min. / 3 feet max.	R
Glazing height	8 feet min.	S

Optional Awning

Depth	3 feet min.	H
Height, ground level clear	8 feet min.	J
Width, length of facade	70% min.	K
Set back from curb	2 feet min.	N



APPENDIX D
ARCHITECTURAL DESIGN GUIDELINES

A STYLE DESCRIBED

This style of architecture displays a classical appearance, but with materials, decorative flourishes and characteristics adapted for a tropical climate. Often considered an eclectic style, it originated in the British-settled isles of the Caribbean and was influenced by Portuguese, Dutch, French, and Spanish colonizations.

West Indies style homes tend to be symmetrical and balanced. Even when brightly painted and embellished, there's a sense of proportion and that every element works together.



Figure 32 29 Lagarza Court , Alys Beach, FL. Architect: Mike Piazza Image:BHH Affiliates LLC

A STYLE DEFINED

1 ROOFS	2 FINISHES & FEATURES	3 DOORS & WINDOWS	4 PORCHES & STOOPS	5 OVERHANGS	6 FOUNDATION
<ul style="list-style-type: none"> The roof is high-pitched to shed tropical rains Roofing materials consist of standing seam or “V” crimp metal, wood or asphalt shingles, or slate. Roof overhangs are quite deep with narrow eaves and exposed rafter tails. Brackets are often used to support the overhang. 	<ul style="list-style-type: none"> West Indie plans are generally symmetrical. The exterior finish is predominantly smooth stucco. Colors tend to be subtle with an emphasis on natural materials and earth tones. Detailing and ornamentation is very simple and tectonic in its usage Floor plans feature outdoor living areas, covered porches and abundance of windows and sliding glass doors to connect to the outdoors. Exterior shutters are another staple West Indies feature. Bahama shutters are hinged at the top of the window and filter sunlight. 	<ul style="list-style-type: none"> Windows and doors are vertically proportioned. Openings for doors and windows are recessed, casting deep shadows and revealing the thickness and solidity of the structure. Doors surrounds are more prominent and sculptural in design. Window surrounds are minimal; when they exist, surrounds are made of stucco or stone. 	<ul style="list-style-type: none"> Columns, posts, railings, brackets, louvered openings, and shutters are all elements definitive of the style Second story balconies and two-story porches are distinguishing features of the West Indie style. 	<ul style="list-style-type: none"> Overhangs are deep and help shade higher windows and walls. 	<ul style="list-style-type: none"> Slab on grade foundations are common.

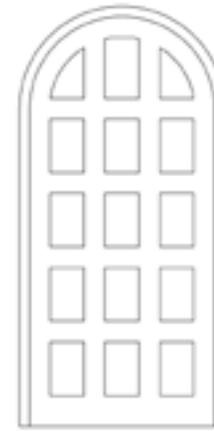


APPROPRIATE WEST INDIES DOORS

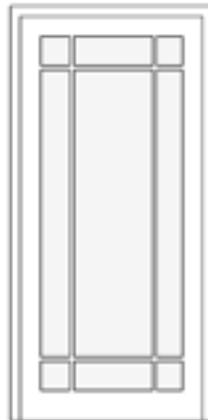
- Doors are of vertical proportions.
- “French Doors” are often a feature of the West Indies Style.
- Doors are mostly wood paneled or with fixed glass insets.
- Fixed glass insets and transoms are often used to enhance the vertical proportions, and invite natural light.
- Louvered wooden doors.



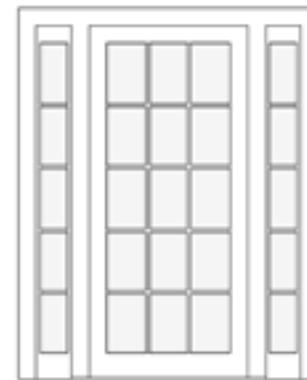
WOODEN PANEL DOOR



ARCHED WOODEN PANELED DOOR



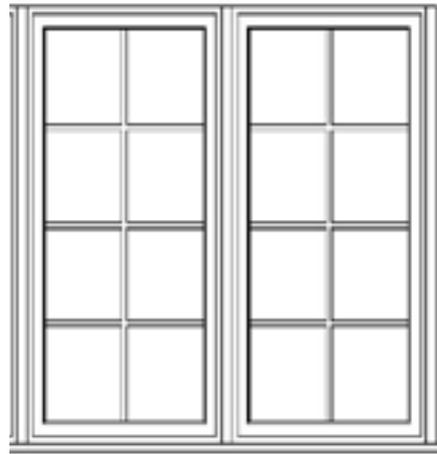
WOODEN DOOR WITH FIXED WINDOW PANES



“FRENCH” DOOR

APPROPRIATE WEST INDIES WINDOWS

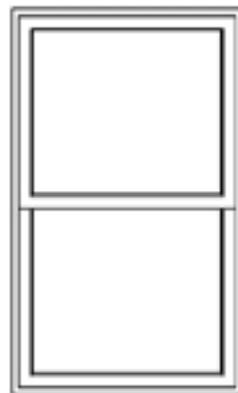
- Windows are of vertical and/or square proportions.
- Openings for doors and windows are deep and cast deep shadows as well as give the impression of thickness and solidity.
- Windows can have divided lights, single lights, and may borrow light configuration from the Florida Bungalow or Craftsman languages.
- Windows are most commonly double-hung or casement.
- Window and door surrounds, when they exist, are made of stucco, stone, or wood.
- Operable wooden shutters and also “Bahama Shutters” compose the elevation



VARIATION OF WOODEN CASEMENT



DOUBLE HUNG WINDOWS WITH TRANSOM LIGHTS



ONE OVER ONE DOUBLE HUNG WINDOWS



DOUBLE HUNG WINDOWS WITH TRANSOM LIGHTS

APPROPRIATE WEST INDIES ROOFS & EXTERIOR SURFACES

- West Indies buildings always feature a smooth stucco exterior finish.
- There is often lower level stucco, but upper level siding can also be applied.
- Roofs high-pitched with a slope of 35-50 degrees with clay or concrete tile, slate tile, standing seam metal, or wood shingles and shakes.
- Exposed rafter tails. Brackets are often used to support the overhang.



STUCCO TREATMENTS ARE TYPICALLY SMOOTH IN TEXTURE



EXPOSED RAFTER TAILS AND BRACKETS WITH DECORATIVE WOODEN RAILINGS AND CARVED WOOD DETAILS



SCULPTED CONCRETE PARAPET WALLS ARE A CHARACTER DEFINING ELEMENT



CLAY TILE IS USED MOST FREQUENTLY FOR ROOFING MATERIAL

GALLERY OF EXAMPLES



Figure 33 Naples, Florida, Kukk Architecture & Design



Figure 35 Alys Beach, FL, William T. Baker Architect



Figure 34 Purdum Residence, Tampa, FL, Cooper Johnson Smith Peterson Architects & Town Planners



Figure 36 Fort Myers, FL, MHK Architecture & Planning

A STYLE DESCRIBED

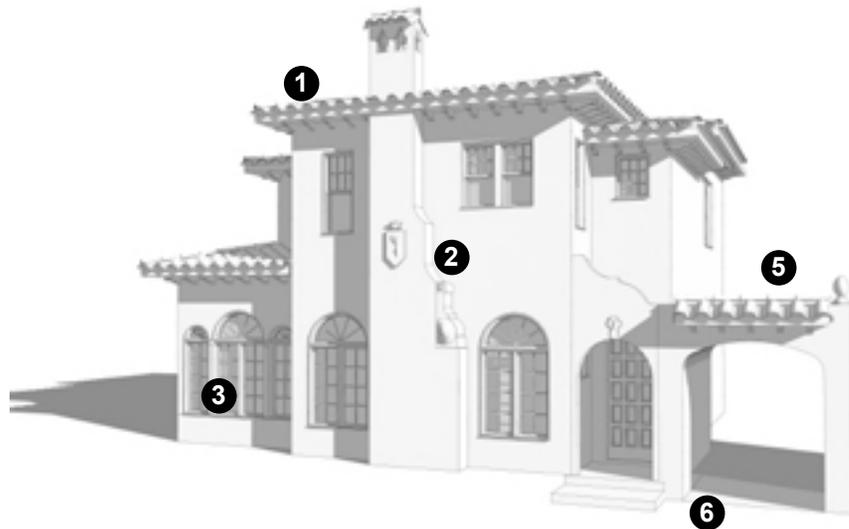
Mediterranean Revival is a design style introduced primarily in Florida and California at the end of the 19th and beginning of the 20th centuries. The style references the architecture of the Mediterranean, especially that of the Beaux-Arts, the Venetian Gothic, and the Spanish and Italian Renaissance. The principal mass of a Mediterranean structure is typically rectangular in plan, with elements such as towers, loggias, porches, balconies, chimneys, and garden walls added for compositional effect. The language of classical architecture is referenced by the use of the Classical Orders (columns, profiles, and details) but Mediterranean Revival does not follow the stricter rules of symmetry and superimposition of the Classical style.



Figure 37 150 Worth Avenue, Palm Beach, FL

A STYLE DEFINED

1 ROOFS	2 FINISHES & FEATURES	3 DOORS & WINDOWS	4 PORCHES & STOOPS	5 OVERHANGS	6 FOUNDATION
<ul style="list-style-type: none"> • Roofs of the primary structure are hipped, gabled, or a combination of both. Roof slopes are shallow and are sloped between 3:12 and 6:12. • Roofing materials consist of barrel tile, Spanish “S” tile, or flat concrete tile. 	<ul style="list-style-type: none"> • Exterior finishes are almost exclusively stucco. Buildings are colored with soft earth tones and contrasting colors frequently highlight recessed areas such as loggias and porches. • It is common to have multiple building volumes and varied interior and exterior spaces. Building massing is irregular, with a variety of shapes and heights. 	<ul style="list-style-type: none"> • Windows and doors are of vertical and/or square proportions with the occasional round, oval or ornamental window used as a facade accent. • Windows have divided lights and are commonly double-hung, single-hung, or casement. Window and door surrounds are minimal and are made of stucco or stone. 	<ul style="list-style-type: none"> • Roof overhangs vary from deep to having no overhang at all. When deep overhangs exist, they are supported by sizable wooden brackets. Roofs without overhangs are finished with a molded cornice. • Defining characteristics of the Mediterranean Revival architecture include attached porches, balconies, courtyards, towers, loggias, and arched openings. 	<ul style="list-style-type: none"> • A “bris-soleil” was introduced, it is a screen over a building that allows breezes to come through but shades the windows from the blistering Florida sun. 	<ul style="list-style-type: none"> • Masonry built with either a stone, brick, or stucco finish.



BALCONIES/TERRACES:

- Use of wrought iron for window and balcony grills, wrought iron sconces
- Both functional and ornamental balconies are common. Often flat roofs serve as terraces.

CHIMNEY:

- Masonry built with either a stone, brick, or stucco finish.
- Flues are commonly roofed or have decorative chimney caps.

II. MEDITERRANEAN REVIVAL

APPROPRIATE MEDITERRANEAN REVIVAL DOORS

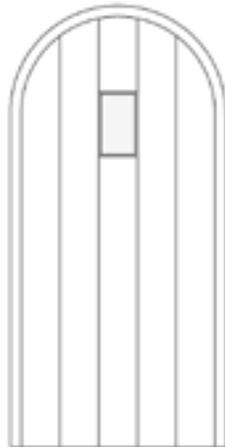
- Mediterranean Revival doors are mainly wooden and frequently have a rusticated treatment.
- They often have multiple fixed glass panes and can be arched.
- They are often plank doors and sometimes have small glass inserts.
- The doors can be large with applied wooden decorative motifs.



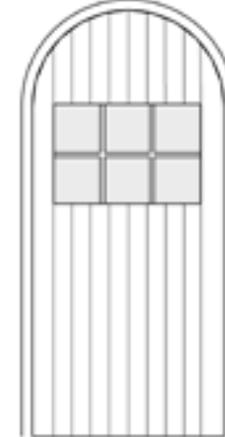
WOODEN PANEL DOOR



ARCHED WOODEN PANELED DOOR



ARCHED WOODEN DOOR WITH FIXED
WINDOW PANES

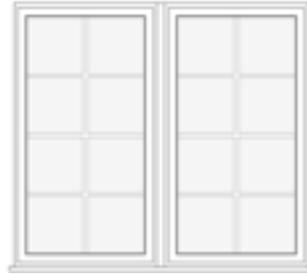


ARCHED WOODEN DOOR WITH FIXED
WINDOW PANES

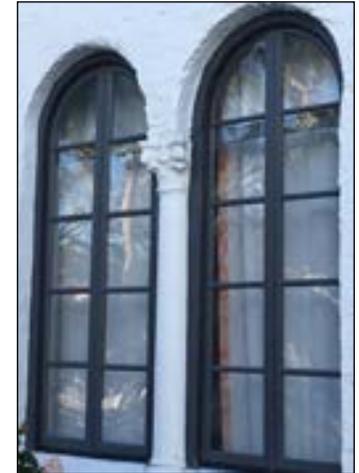
II. MEDITERRANEAN REVIVAL

APPROPRIATE MEDITERRANEAN REVIVAL WINDOWS

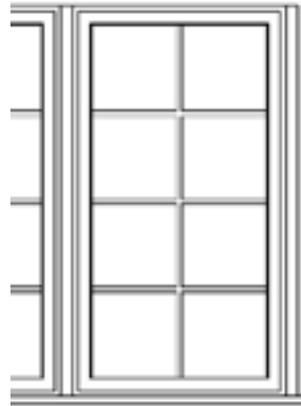
- Mediterranean Revival windows are primarily double-hung, single-hung, or casement type windows.
- Traditionally Mediterranean Revival windows are made of wood.
- Mediterranean Revival windows can be square or vertically proportioned and often might be arched or a decorative shape.
- These windows are inset deep into the exterior wall creating deep sill and shadow lines.
- These windows may or may not be operable (typically windows of an ornamental shape might be fixed).



MULTI-LIGHT WOODEN CASEMENT
WINDOWS



ARCHED WOODEN CASEMENT WINDOWS



CASEMENT WINDOWS WITH VARIATION OF
TRANSOM LIGHTS ABOVE



ONE OVER ONE, AS SINGLE HUNG OR
DOUBLE HUNG WINDOWS

II. MEDITERRANEAN REVIVAL

APPROPRIATE MEDITERRANEAN REVIVAL ROOFS & EXTERIOR SURFACES

- Mediterranean Revival buildings almost always have a medium to heavily textured stucco exterior finish.
- There are often masonry or stucco treatments around windows and doors.
- Mediterranean Revival roofs are almost always clay barrel tile with occasional flat roof sections that might be accessible from an interior room.



STUCCO TREATMENTS ARE TYPICALLY
MEDIUM TO ROUGH IN TEXTURE



DECORATIVE STUCCO DETAILS AROUND
WINDOWS & DOORS



CLAY BARREL TILE IS ONE OF THE MOST
CHARACTER DEFINING ELEMENTS



BARREL TILE IS USED ON THE MAIN ROOF,
PORCHES, & BRACKETED OVERHANGS

II. MEDITERRANEAN REVIVAL

GALLERY OF EXAMPLES



Figure 38 *Via Mizner, Palm Beach, FL Architect: Addison Mizner.*



Figure 40 *Everglades Club, Palm Beach, FL Architect: Addison Mizner.*



Figure 39 *Old Police Station, Honolulu, Hi.*



Figure 41 *AM Trust Bank, Coral Gables, FL.*

A STYLE DESCRIBED

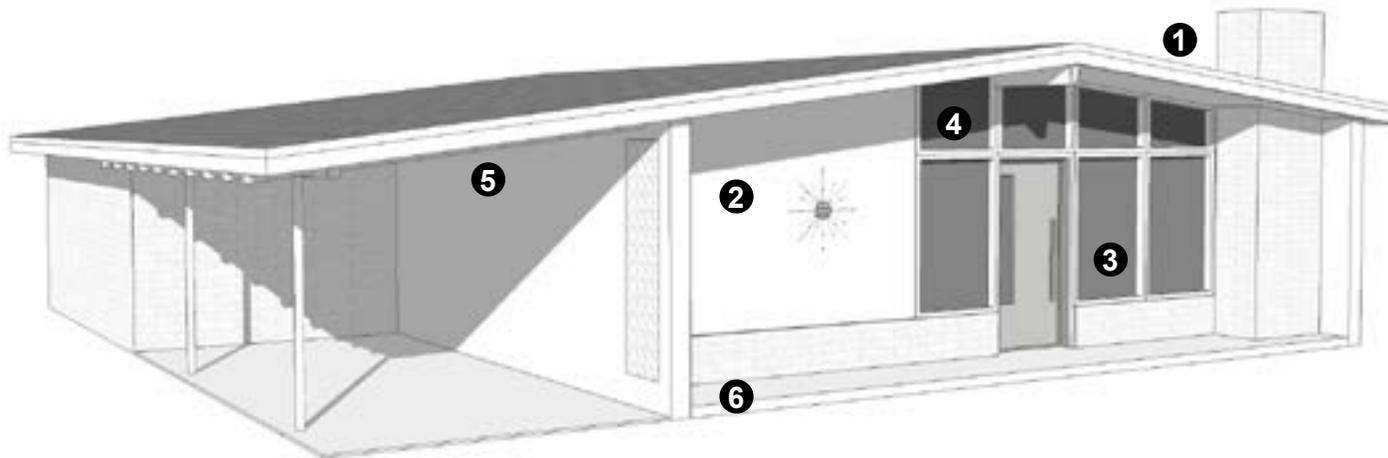
Mid-century Modern as a building style was popular on many levels: for major tourist resorts and hotels, multi-family condominiums, commercial structures, garden-style apartment buildings, and single-family homes. The overriding characteristic of mid-century buildings is that of experimentation; both with construction materials and applied architectural features. Designs very much reflected a move away from the stuffy and traditional, with elements such as angled rooflines and pylons, geometric shapes in railings, curved eaves, parabolic arches, and glass curtain walls all making reference to space-age technologies and the new advancements in transportation design. There was a heavy use of glass and poured concrete, and facades (a side of a building) typically featured more than one material, such as stucco, stone, brick, masonry sculptural elements, mosaic tiles and metal. The intent was to utilize the various materials and projecting elements in order to break up what would have otherwise been a largely uniform (and boring) building façade.



Figure 42 Haydon Burns Library, Jacksonville, FL, Architect: Taylor Hardiwick

A STYLE DEFINED

1 ROOFS	2 FINISHES & FEATURES	3 DOORS & WINDOWS	4 PORCHES & STOOPS	5 OVERHANGS	6 FOUNDATION
<ul style="list-style-type: none"> • Roof lines became very asymmetrical, with one slope of a gable roof being longer than another, or imitating the wings of an airplane (sometimes called a butterfly or “v” roof). • Otherwise, most roofs for mid-century buildings were flat or shed roofs. 	<ul style="list-style-type: none"> • Decorative elements typical in a mid-century building include planter and landscape walls, usually of brick facing, rounded eaves, large pylons or prosceniums at the entrances, and the use of lally columns (or beanpoles) instead of traditional columns. 	<ul style="list-style-type: none"> • Windows were typically metal casement, awning, or jalousie. Architects found new ways to wrap the windows around corners, to emphasize the fact that corner supports were no longer needed with the new building technology. • A new feature called window boxes were introduced - a projected concrete element around one or several windows in a row (ribbon windows) that served to create more visual interest on the building. 	<ul style="list-style-type: none"> • A key feature of the mid-century garden-style apartment building was the drastic change to a central courtyard or pool area, with all the apartment front doors opening up onto that courtyard, connected by open air corridors or catwalks. 	<ul style="list-style-type: none"> • A “bris-soleil” was introduced, it is a screen over a building that allows breezes to come through but shades the windows from the blistering Florida sun. 	<ul style="list-style-type: none"> • Mid Century homes had block, stone, brick and concrete foundations.



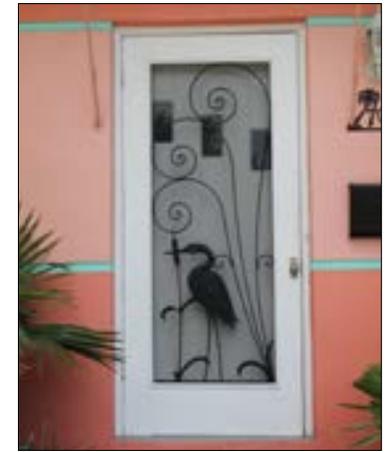
III. MID-CENTURY MODERN

APPROPRIATE MID-CENTURY MODERN DOORS

- Typical Mid-Century Modern doors include wood multi-paneled, full light jalousie, French door with five lights, flush door with applied trim, flush door with lights in an asymmetrical pattern, and decorative screen doors.
- Often front doors may be surrounded by geometrically interesting fixed plate glass windows.



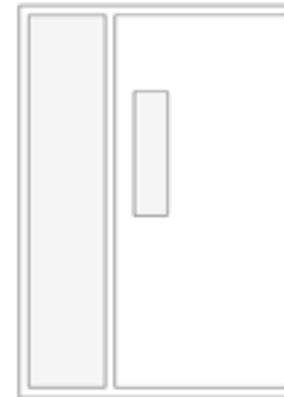
THREE PANEL WOOD DOOR



THREE-LIGHT DOOR WITH DECORATIVE SCREEN DOOR



FIVE HORIZONTAL LIGHT DOOR



FLUSH DOOR WITH ASYMMETRICAL LIGHT AND SIDELIGHTS

III. MID-CENTURY MODERN

APPROPRIATE MID-CENTURY MODERN WINDOWS

- Mid-Century Modern windows are typically steel casement, fixed-pane, aluminum awning and periodically jalousie type windows.
- Mid-Century windows are almost always metal with square or horizontal lights (panes).
- The Mid-Century Modern windows can be vertical, square, or horizontal in their proportion. Window openings and details often emphasize the horizontality of this style.
- Mid-Century Modern structures can also include clerestory windows.



AWNING WINDOWS



GEOMETRIC FIXED-PANE GLASS



FIXED-PANE GLASS WITH SINGLE PANE CASEMENT



GROUPED FIXED-PANE WINDOWS WRAPPING A CORNER WITH A PIPE COLUMN

III. MID-CENTURY MODERN

APPROPRIATE MID-CENTURY MODERN ROOFS & EXTERIOR SURFACES

- Mid-Century Modern roofs are typically very long, low-pitched gable roofs or asymmetrical and clerestory-type roofs. They may also be flat or a butterfly type roof that has inverted slopes.
- Mid-Century Modern roofs are usually finished with shingles or concrete tiles, rolled roofing, pitch and gravel, or other flat roof materials.



USE OF MULTIPLE MATERIALS INCLUDING STUCCO, BRICK, GLASS, & METALLIC PANELS

ASYMMETRICAL SHED ROOF AND STAIRWELL WITH TUBE RAILINGS & A BREEZE BLOCK WALL



GEOMETRIC FIXED-PANE GLASS SURROUND A PANELED FRONT DOOR WITH A SINGLE SIDE LIGHT & CARPORT



CLERESTORY WINDOW

GALLERY OF EXAMPLES



Figure 43 Seacoast Bank, Stuart, FL



Figure 45 Cocoon House, Sarasota, FL, Paul Rudolph



Figure 44 Sarasota High School, Sarasota, FL, Paul Rudolph



Figure 46 Coral Gables, FL, photographed by Phillip Pessar

A STYLE DESCRIBED

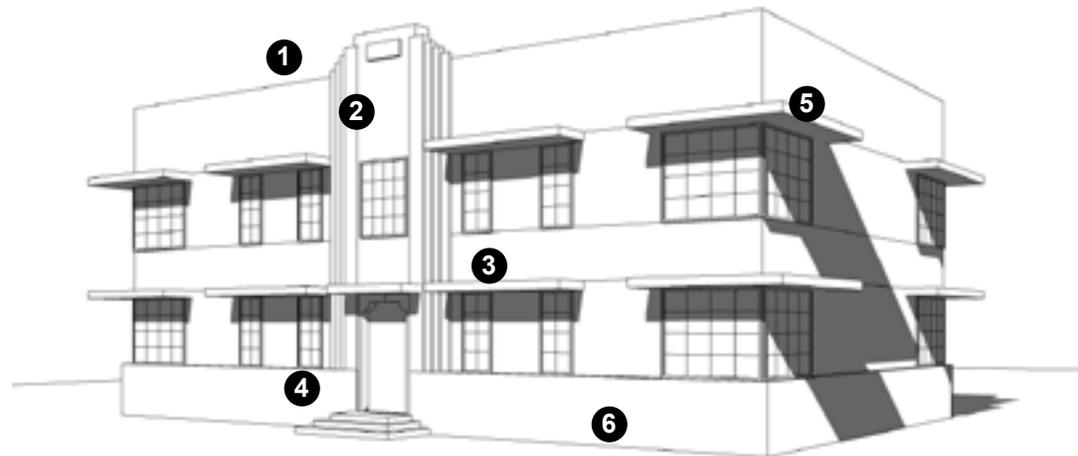
Art Deco is an influential modern design style that first appeared in France post WWI. It is an eclectic style, borrowing traditional craft details and incorporating influences from the streamlined machine age. The popularity of the Art Deco waned post WWII, although it is often referenced in modern architectural design. While stone typically was used in northern examples of the Art Deco, masonry and stucco with vivid color highlights are common in Florida.



Figure 47 *The Colony Hotel, Miami Beach, FL, Henry Hohouser*

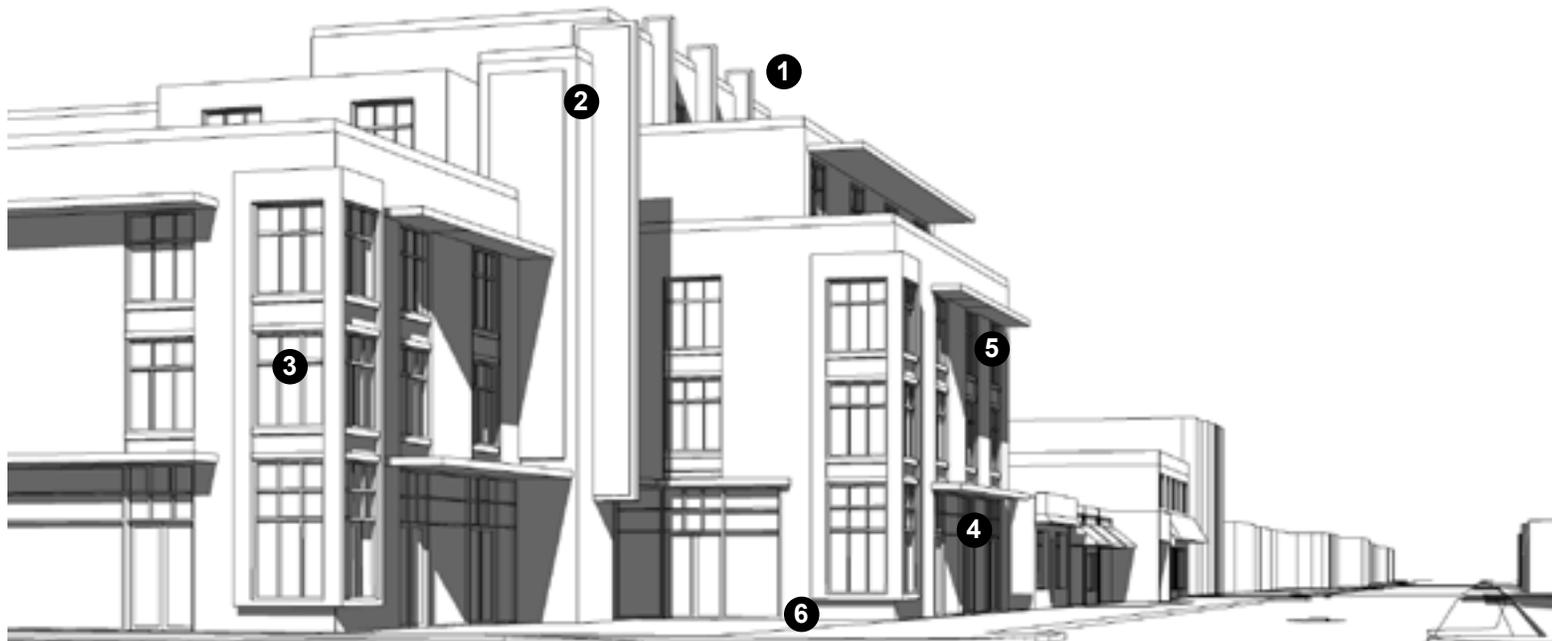
A STYLE DEFINED

1 ROOFS	2 FINISHES & FEATURES	3 DOORS & WINDOWS	4 PORCHES & STOOPS	5 OVERHANGS	6 FOUNDATION
<ul style="list-style-type: none"> • Roofs of the primary structure are flat with concealing parapets. • Parapets are embellished with ornamentation. 	<ul style="list-style-type: none"> • Abstract figures and geometries often adorn the exterior of an Art Deco building. In Florida, the flora and fauna native to the region are often represented in panels between windows, or in the signage, parapet, and base of the building. • Corners are often rounded and horizontal lines are expressed. Individual elements such as towers and signage are vertically oriented. • Exterior finishes in the Florida Art Deco style are almost exclusively stucco and colored with light, neutral, or pastel colors. Ornamentation is often highlighted to provide a contrast to building mass. 	<ul style="list-style-type: none"> • Openings for doors and windows are recessed, casting deep shadows revealing the thickness and solidity of the structure. • Windows are commonly located at corners, emphasizing the horizontal composition of the building facade and expressing modern building structural advancement. • Garage doors were wood recessed panel, vertical plank, or horizontal flush or ribbed. 	<ul style="list-style-type: none"> • Mostly masonry with stucco finishes, metal is occasionally used. • Ground or upper floor terraces are typical and use masonry walls or horizontal tube railings. 	<ul style="list-style-type: none"> • Deep “eyebrow” overhangs placed over the windows. 	<ul style="list-style-type: none"> • The Art Deco house sits on a masonry base.



COMMERCIAL USE

- In Florida, Art Deco was most often used on apartment buildings, hotels, and commercial buildings, but the use of this style on residential homes was far less frequent. Local architects would put their own spin on the style and would often pay tribute to the local tropical setting: designs featured pelicans, palm trees, flamingos and ocean motifs.
- Art Deco buildings have smooth walls, usually finished in stucco, and typically have flat roofs, sometimes surrounded by a parapet. There is a dizzying array of motifs (or patterns) used to decorate the facades. Zig zags, sunbursts, chevrons, botanical designs, and all kinds of geometric patterns were typically found around doorways, on the surfaces of projecting pylons and towers, pediments, and at the top of the buildings or on the parapet.

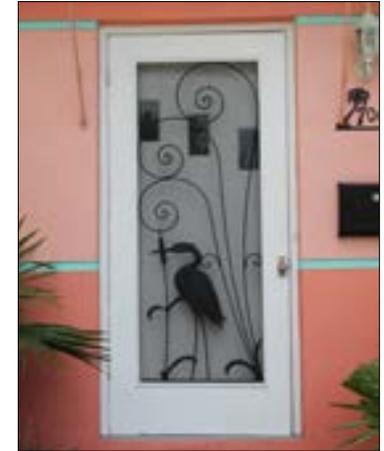


APPROPRIATE ART DECO DOORS

- Art Deco doors are typically flush or plank wooden doors with porthole windows.
- Art Deco doors are often French with 5 horizontal windows.
- These doors may also be flush with applied trim and often include decorative screen doors.



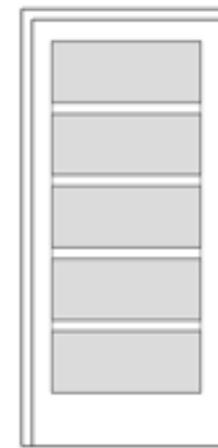
VARIATION ON THE PANEL DOOR WITH FIXED WINDOWS



THREE-LIGHT DOOR WITH DECORATIVE SCREEN DOOR



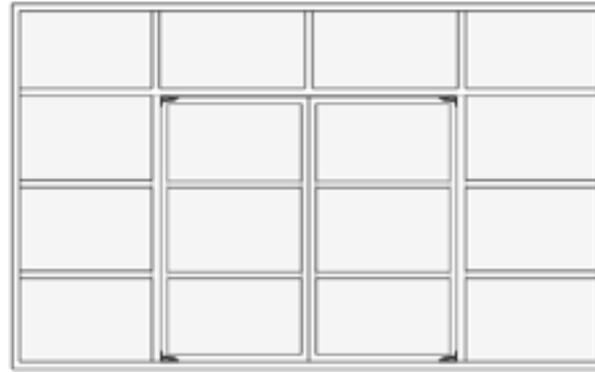
WOOD PLANK DOOR WITH PORTHOLE WINDOW



FRENCH DOOR WITH FIVE HORIZONTAL LIGHTS

APPROPRIATE ART DECO WINDOWS

- Art Deco windows are typically casement, fixed, and awning type windows. Jalousie type windows were occasionally used. Glass block and round porthole windows were also frequently used as accent windows.
- Art Deco windows are almost always metal with square or horizontal lights (panes).
- The windows can be vertical, square, or horizontal in their proportion.
- Art Deco buildings do not typically have applied operable shutters so storm protection is not a permanent fixture.
- A unique and prominent feature is corner windows which are typically formed with casement or awning window types.
- Often eyebrow overhangs were provided over the windows.



FIXED AND CASEMENT WINDOWS



AWNING WINDOWS



GROUPED CASEMENT WINDOWS WRAPPING A CORNER WITH A PIPE COLUMN AT THE EDGE



ROUNDED HORIZONTAL PANE, FIXED GLASS.



PORTHOLE WINDOW

APPROPRIATE ART DECO ROOFS & EXTERIOR SURFACES

- Art Deco structures are almost exclusively smooth stucco with vertical banding and stucco fluting details.
- There are often cantilevered masonry “eyebrows” that protrude over windows and doors.
- Art Deco typically emphasizes verticality while Streamline Moderne places emphasis on the building’s horizontality.
- A Unique feature in Art Deco architecture are round porthole windows.
- Glass block flanking either side of the entry is a common element in Art Deco architecture.
- Flat roofs are typical.



LARGE PORTHOLE WINDOW & CANTILEVERED EYEBROWS OVER THE WINDOWS AND ENTRY



CANTILEVERED “EYEBROWS” OVER THE WINDOWS AND DOORS



PORTHOLE OPENINGS IN DECORATIVE WING WALLS



SMOOTH STUCCO WITH STUCCO BANDING AND FLUTING



GALLERY OF EXAMPLES



Figure 48 Office Building, Miami, FL



Figure 50 Albion Hotel, Miami Beach, FL



Figure 49 The Carlyle, Miami Beach, FL



Figure 51 The BreakWater, Miami Beach, FL

A STYLE DESCRIBED

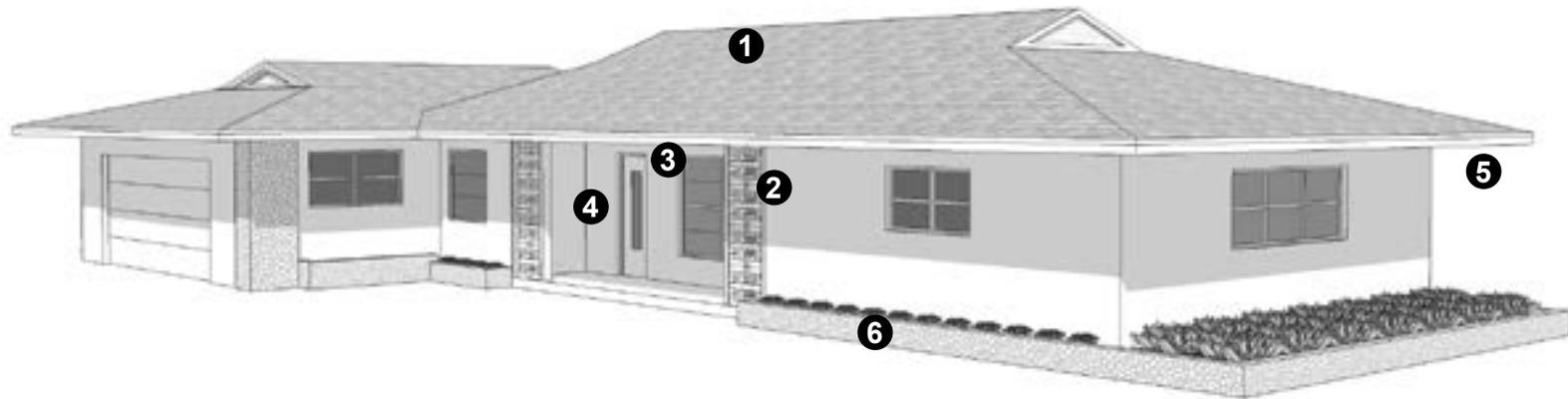
Since the ranch home's main emphasis was to connect indoor living with the outdoors, they are very shallow, sometimes only one room deep, but spread out horizontally. Many are L or U-shaped in plan, with a courtyard in the center. This is the first style to actively utilize the sliding glass door as a key component of the architecture. Ranch houses are generally one story, and most include an attached garage or carport. One end of the home, or sometimes both ends, protrudes out from the front of the house. The roof is low-pitched (meaning it is not steep) and features deep eaves. To convey its historic setting of being on an open plain or in the vast western wilderness, many of the materials used are rustic in nature and were left unpainted. Wall materials can vary from clapboard, stucco, or brick. Those ranch homes that were not completely made of brick sometimes had a brick veneer feature on the front of the house, or brick planters.



Figure 52 Lake Worth Beach, FL

A STYLE DEFINED

1 ROOFS	2 FINISHES & FEATURES	3 DOORS & WINDOWS	4 PORCHES & STOOPS	5 OVERHANGS	6 FOUNDATION
<ul style="list-style-type: none"> The roof is low-pitched (meaning it is not steep) and features deep eaves. 	<ul style="list-style-type: none"> The plan of a ranch house is rambling, long and linear, and lays out more horizontally. Plan is generally asymmetrical. Predominantly one-story. Minimal decoration. Often features a U-shaped or L-shaped house plan around an outdoor patio or courtyard. Contrasting brick veneer or other rustic materials on facade. Ranch houses often use more open plans to arrange interior rooms and space. Floor to ceiling heights are lower than other styles. 	<ul style="list-style-type: none"> Doors are typically wood multi-panel, full light jalousie window, or flush with asymmetrical fixed lights. Windows are typically aluminum awning, jalousie, or fixed pane picture windows. 	<ul style="list-style-type: none"> May have geometric porch posts or wrought iron supports. Courtyards or internal patios are often featured. 	<ul style="list-style-type: none"> Overhangs are deep and help shade higher windows and walls. 	<ul style="list-style-type: none"> Slab on grade foundations are common.



APPROPRIATE
RANCH DOORS

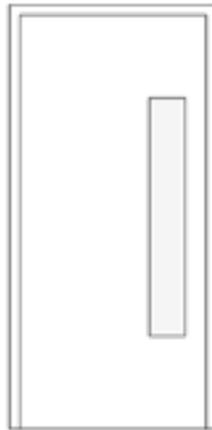
- Ranch doors are typically wood multi-panel, full light jalousie, and French door with five horizontal lights. It is also common for Ranch structures to have flush wooden doors with applied trim or with lights in an asymmetrical pattern.
- There are also examples where Ranch homes will have oversized and double doors at the entry.



THREE PANEL WOOD DOOR



THREE-LIGHT DOOR WITH DECORATIVE
SCREEN DOOR



FLUSH WOOD DOOR WITH SINGLE
ASYMMETRICAL LIGHT



FRENCH DOOR WITH FIVE HORIZONTAL
LIGHTS

APPROPRIATE RANCH WINDOWS

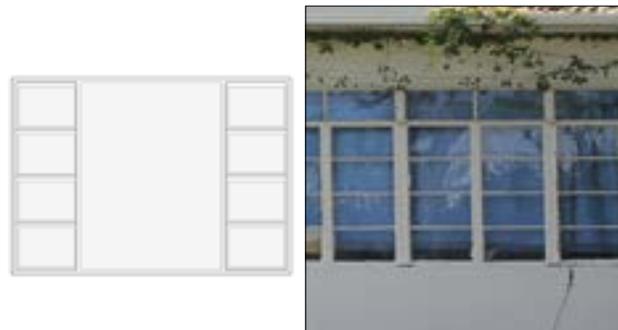
- Ranch windows are primarily aluminum awning, jalousie, steel casement, or fixed-pane picture window.
- While the actual window units in the Ranch style may be square or vertical in their proportion, typically the windows are grouped in a manner to accentuate the horizontal composition of the structure.
- Ranch windows may have minimal sill and header detailing, typically masonry or brick.
- Ranch windows may have shutters but typically they are not sized to the window openings and are mostly decorative features.



ALUMINUM AWNING WINDOW



JALOUSIE WINDOW



FIXED-PANE & STEEL CASEMENT WINDOWS



FIXED-PANE PICTURE WINDOW

APPROPRIATE
RANCH ROOFS &
EXTERIOR SURFACES

- Ranch roofs are often long, low-sloping gable type, flat, or asymmetrical sloping shed.
- Ranch roofs are typically flat white concrete tiles, asphalt shingle, or pitch and gravel type treatments.
- The base building of the Ranch style is typically finished in smooth stucco however many accent materials are common.
- A variety of materials are typically used as architectural accents including brick, board and batten, slumped brick, and bas-relief panels.



SMOOTH STUCCO WITH BRICK ACCENTS



FLAT WHITE CONCRETE TILES



THIS RANCH HOME HAS ASPHALT SHINGLES & A DEEP PORTE-COCHERE



SMOOTH STUCCO WITH A CONTINUOUS BRICK BASE

GALLERY OF EXAMPLES

